



**BOARD OF ZONING ADJUSTMENT  
AGENDA  
MEETING OF JUNE 9, 2020, 3:00 PM**  
Council Chambers, City Hall South, 1501 Truxtun Avenue

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**1. ROLL CALL**

**2. PUBLIC STATEMENTS:**

**3. MINUTES**

- a. Approval of Minutes for the May 12, 2020 regular meeting.  
Staff recommends approval.

**4. CONSENT CALENDAR PUBLIC HEARINGS**

Ward 5

- a. **Conditional Use Permit No. 20-0069:** McIntosh & Associates, on behalf of Dignity Health-Mercy Hospital Southwest, is proposing a conditional use permit to allow a helipad incidental to an existing hospital in the C-2 (Regional Commercial Zone) district, located at 551 Shanley Court. Notice of Exemption on file.  
Staff recommends approval.

**5. PUBLIC HEARINGS**

**6. STAFF COMMUNICATIONS**

**7. BOARD STATEMENTS**

**8. ADJOURNMENT**

Paul Johnson  
Planning Director



# COVER SHEET

## PLANNING DEPARTMENT

### STAFF REPORT

**MEETING DATE:** June 9, 2020

**ITEM NUMBER:** MINUTES3.(a.)

**TO:** Board of Zoning Adjustment

**FROM:** Paul Johnson, Planning Director

**PLANNER:** Kassandra Gale, Principal Planner

**DATE:**

**WARD:**

**SUBJECT:** Approval of Minutes for the May 12, 2020 regular meeting.

**APPLICANT:**

**OWNER:**

**LOCATION:**

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**STAFF RECOMMENDATION:**

Staff recommends approval.

**ATTACHMENTS:**

Description	Type
☐ Minutes	Backup Material



# BOARD OF ZONING ADJUSTMENT MINUTES

Meeting of May 12, 2020, 3:00 p.m.  
Council Chambers, City Hall, 1501 Truxtun Avenue

		<u>ACTION TAKEN</u>
<b>REGULAR MEETING</b>		
1.	<b>ROLL CALL</b>  Present: Members Fick, Kitchen, and Patteson Absent:	
2.	<b>PUBLIC STATEMENTS</b>  None.	
3.	<b>MINUTES</b>  a. Approval of minutes of the April 14, 2020 regular Board of Zoning Adjustment meeting.  <i>Motion by Member Kitchen to approve. Motion passed.</i>	
4.	<b>CONSENT CALENDAR PUBLIC HEARINGS</b>	
(Ward 4)	a. Conditional Use Permit No. 20-0031	Moved to 5.a
	b. Conditional Use Permit No. 20-0069: McIntosh & Associates, on behalf of Dignity Health-Mercy Hospital Southwest, is proposing a conditional use permit to allow a helipad incidental to an existing hospital in the C-2 (Regional Commercial Zone) district, located at 551 Shanley Court. Notice of Exemption on file.	Continued to June 9, 2020
	c. Conditional Use Permit No. 20-0072: Sequoia Deployment Services, Inc., on behalf of Verizon Wireless, is proposing a conditional use permit to allow a 60-foot tall monopine wireless telecommunication facility with ancillary equipment in the E (Estate One-Family Dwelling Zone) district, located at 6025 Old Farm Road. Notice of Exemption on file.	Resolution 20-05
	<b>Public hearing opened. Agenda Item 4.a was removed by the public from the Consent Calendar Public Hearings. Public hearing closed.</b>	
	<i>Motion by Member Fick to continue Item 4.b to June 9, 2020, and approve consent calendar Item 4.c. Motion passed.</i>	

**REGULAR MEETING****ACTION  
TAKEN****5. PUBLIC HEARINGS**

a. Conditional Use Permit No. 20-0031: Sean Nourani is proposing a conditional use permit to allow conversion of an existing hotel into a residential care facility in the C-2 (Regional Commercial Zone) district, located at 818 Real Road. Notice of Exemption on file.

**Resolution  
20-04**

Staff presentation given. Public hearing opened. One person spoke in favor of the project, but requested the site be properly maintained and not left to fall in disrepair. Public hearing closed. Board Members deliberated.

*Motion by Member Kitchen to approve the project with the addition of Condition II.7 All masonry walls on site shall be maintained in accordance with City standards for the duration of the project. Motion passed.*

**6. STAFF COMMUNICATIONS**

Planning Director Johnson recognized Kassandra Gale as the new Principal Planner who will be taking over as secretary for the Board of Zoning Adjustment.

**7. BOARD STATEMENTS**

Member Kitchen welcomed Kassandra Gale.

**8. ADJOURNMENT**

Chair Patteson adjourned the meeting at 3:19 p.m.

Marisa Iturralde, Recording Secretary

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Paul Johnson  
Planning Director





# COVER SHEET

## PLANNING DEPARTMENT

### STAFF REPORT

**MEETING DATE:** June 9, 2020

**ITEM NUMBER:** Consent Calendar Public Hearings4.(a.)

**TO:** Board of Zoning Adjustment

**FROM:** Paul Johnson, Planning Director

**PLANNER:** Kassandra Gale, Principal Planner

**DATE:**

**WARD:** Ward 5

**SUBJECT:**

**Conditional Use Permit No. 20-0069:** McIntosh & Associates, on behalf of Dignity Health-Mercy Hospital Southwest, is proposing a conditional use permit to allow a helipad incidental to an existing hospital in the C-2 (Regional Commercial Zone) district, located at 551 Shanley Court. Notice of Exemption on file.

**APPLICANT:** McIntosh & Associates

**OWNER:** Dignity Health-Mercy Hospital Southwest

**LOCATION:** 551 Shanley Court

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**STAFF RECOMMENDATION:**

Staff recommends approval.

**ATTACHMENTS:**

Description	Type
☐ Staff Report	Staff Report
☐ Resolution CUP No. 29-0069	Resolution
☐ Attachment - Public Comment	Backup Material
☐ Attachment - Noise Study Interior Noise Levels	Backup Material
☐ Attachment - Noise Study Nearby Sensitive Receptors	Backup Material



**RECOMMENDATION:** Adopt Resolution and suggested findings **APPROVING** Conditional Use Permit No. 20-0069 as depicted in the project description and subject to the listed conditions of approval.

## PROJECT SUMMARY:

This project was scheduled for consideration on May 12, 2020; however, was continued to today's meeting in order for the applicant to provide clarification on documents related to the project. The project is a request for a conditional use permit to allow a helipad incidental to an existing hospital in a C-2 (Regional Commercial Zone) district. See Figures 2 and 3.

Figure 2. Site Aerial

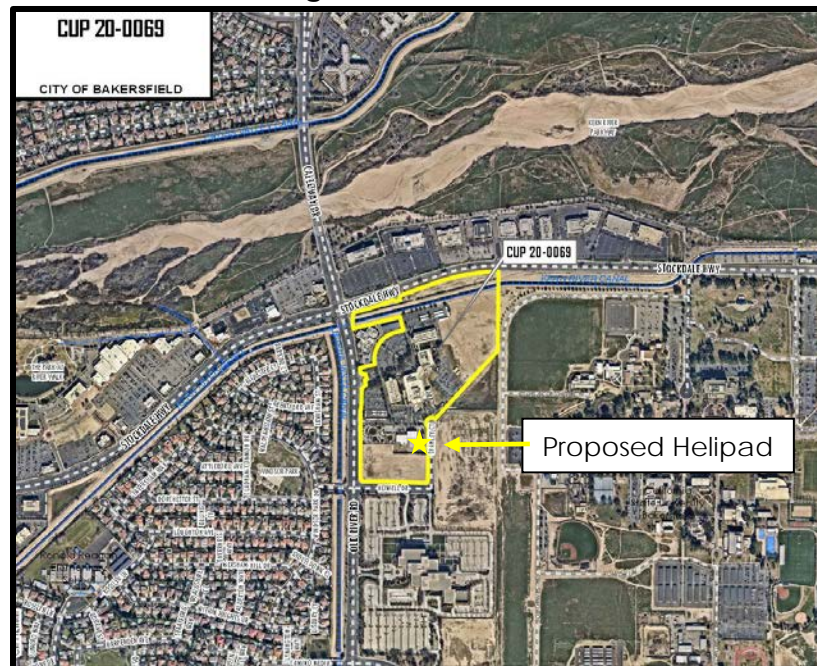
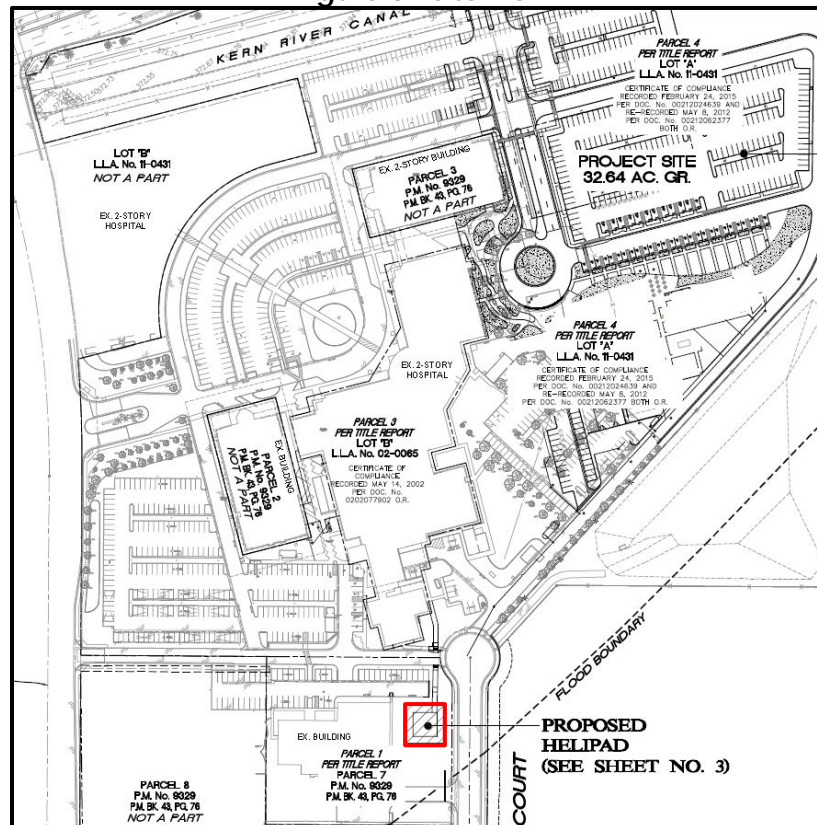


Figure 3. Site Plan



## SURROUNDING LAND USES:

The 2-acre project site is developed with accessory medical buildings and is part of a larger 47 acres for an adjacent hospital, including vacant areas for future expansion. The site and surrounding property's General Plan land use designation, zoning classification, and land use are specified in Table 1:

Table 1. Surrounding Land Uses			
LOCATION	GENERAL PLAN	ZONE DISTRICT	EXISTING LAND USE
Site	OC & P	C-2 and C-O	Hospital
North	OC	C-O	Kern River Canal & Offices
East	PS	R-1 & R-3	University & Vacant Land
South	OC	C-O	Office Building & Parking Lot
West	LR and LMR	R-1	Buena Vista Canal & Single Family Residential
<b>General Plan Key</b> OC: Office Commercial P: Public Facilities PS: Public & Private Schools LR: Low Density Residential LMR: Low Medium Density Residential		<b>Zone District Key</b> C-2: General Commercial C-O: Professional and Administrative Office R-1: Single-Family Dwelling R-3: Multiple-Family Dwelling	

## ANALYSIS:

Hospitals are a permitted use in the C-2 zone; however, helipads require approval of a conditional use permit. According to the applicant, in order to provide world-class emergency services, the hospital requires a helipad to be able to accept patients who require air transport during an emergency. The hospital and helipad would provide important emergency medical services to Bakersfield and surrounding areas.

**Helipad Design.** As proposed, the helipad would be located south of the hospital raised three feet above ground level. The paved helipad is designed as a 40 feet x 40 feet (1,600 square feet) concrete pad. The helipad has been designed to comply with the California Department of Transportation/Aeronautics Division, and the applicant received written confirmation that they can continue to apply for the "State Heliport Permit". See Figure 4.

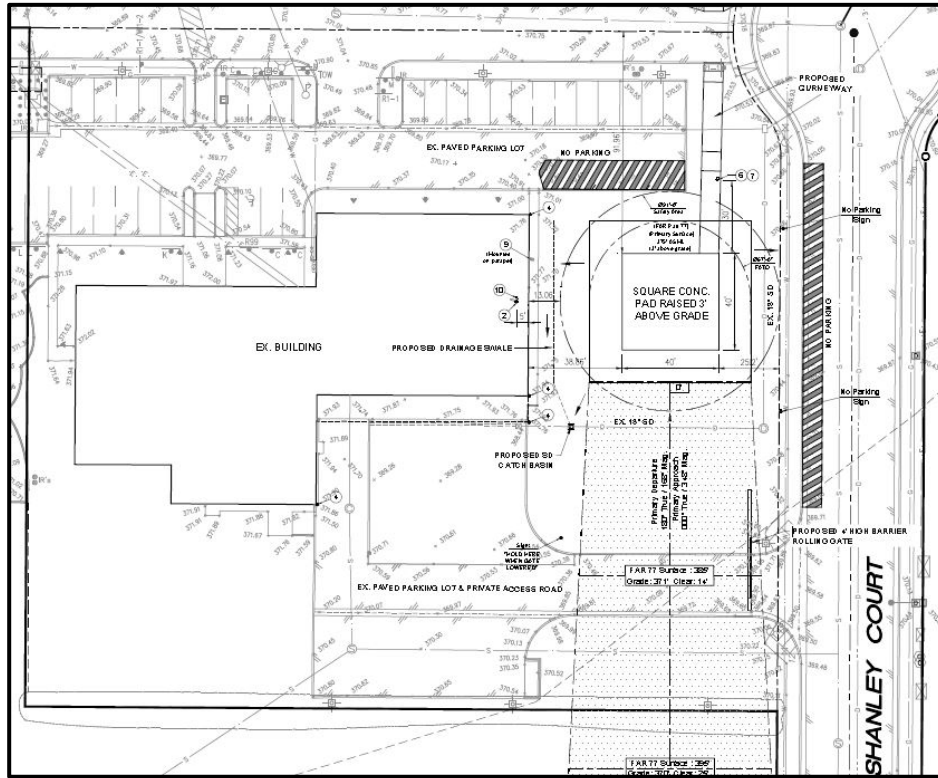
**Flight Schedule.** Flights will not be restricted to certain hours, as emergencies happen sporadically. There will be an average number of two anticipated flights per day, mostly for outgoing patients. The helicopter will only park on the heliport long enough for on and off loading of the patient(s).

**Flight Path.** The flight path will be restricted from/to the south for approach/departure. This will require the helicopter to travel over commercially-zoned property and not residentially-zoned property. See Figures 5 and 6.

**Maintenance.** Helicopter maintenance will primarily occur at the airport where the helicopter is stationed. On-site maintenance will only occur on the helipad during emergency repairs for flight safety.



### Figure 4. Helipad



### Figure 5. Flight Path

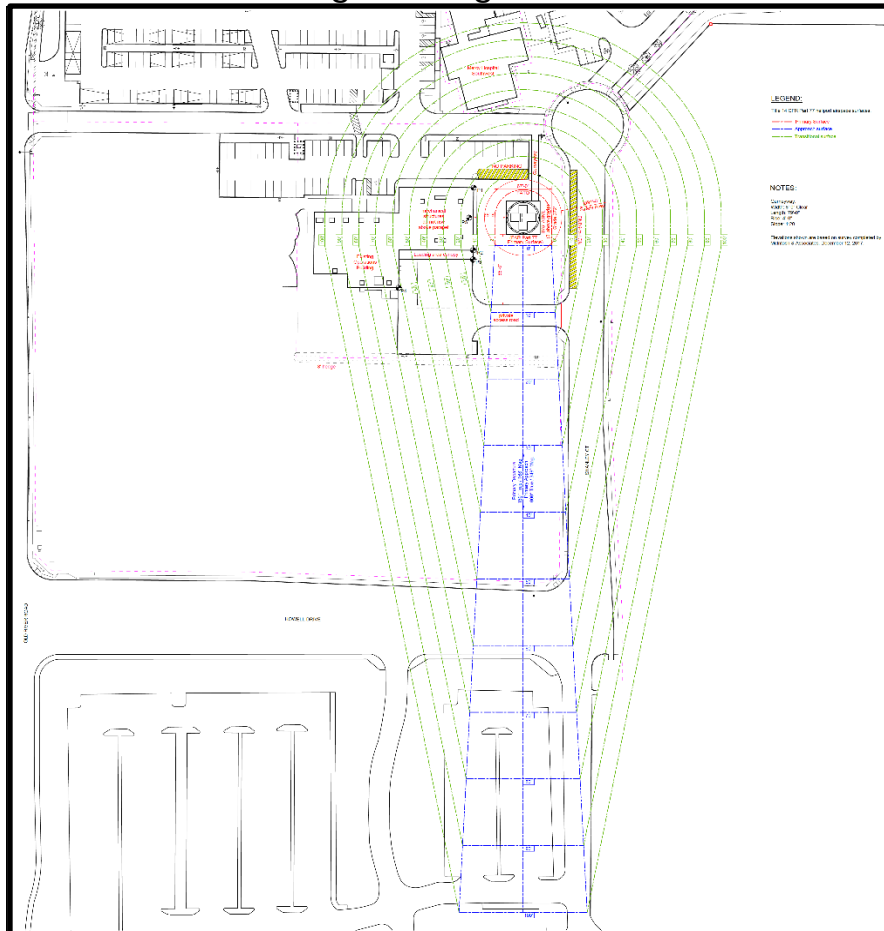
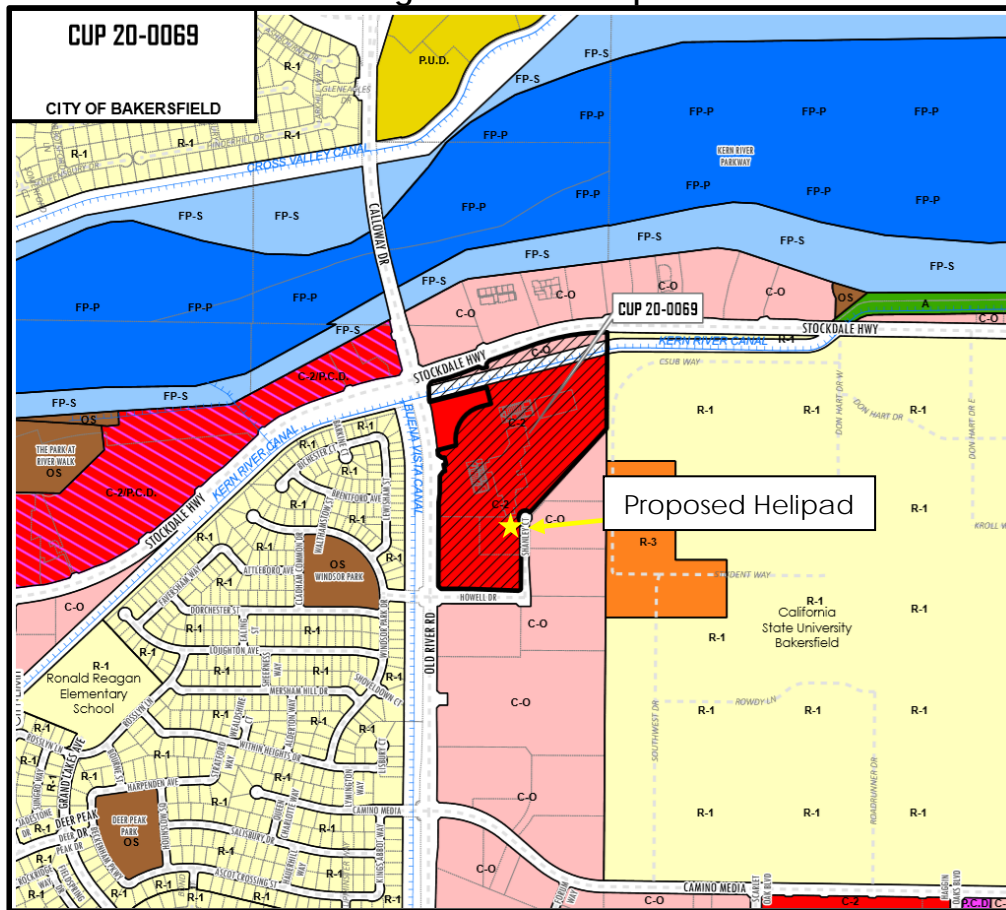


Figure 6. Zone Map



**Noise (Bakersfield General Plan Noise Element).** Although there will only be an average of two anticipated helicopter flights per day, it is recognized that there will be an inherent noise increase with arrival and departure. All commercial uses shall be designed or arranged so that they will not subject noise-sensitive land uses to exterior noise levels in excess of 65 dB CNEL and interior noise levels in excess of 45 dB CNEL, and that impacts on noise-sensitive uses shall not exceed the performance standards in Table VII-2 of the Metropolitan Bakersfield General Plan, Noise Element. See Bakersfield General Plan, Noise Element Table VII-2.

TABLE VII-2			
NOISE LEVEL PERFORMANCE STANDARDS*			
Exterior Noise Level Standards			
Category	Cumulative Number of minutes in any one-hour time period	Daytime 7 a.m. to 10 p.m.	Nighttime 10 p.m. to 7 a.m.
1	30	55	50
2	15	60	55
3	5	65	60
4	1	70	65
5	0	75	70

\* Each of the noise level standards specified in this table shall be reduced by five (5) dB(A) for pure tone noises, noises consisting primarily of speech or music, or for recurring impulsive noises. These noise level standards should be applied at a residential or other noise-sensitive land use and not on the property of a noise-generating land use.

**Noise (Adjacent Hospital).** A noise study was prepared to address the interior noise levels for the Dignity Health, Mercy Hospital Southwest project as it relates to exterior noise intrusion set forth by the California Green Building Code (CALGreen), and Noise Element of the Bakersfield General Plan. The study observed a helicopter landing on site. Although the study noted that helicopter landing/takeoff noise may result in a percentage of patients that experience sleep disturbance, the interior noise levels, with closed windows and mechanical ventilation as required by Code, will conform to CNEL 45 in locations where the exterior noise exposure is CNEL 65 or lower. Although the hospital is not subject to California Building Code (CBC), the study recommends using the CBC as guidance for the criterion for in-patient comfort. See Figure 7.

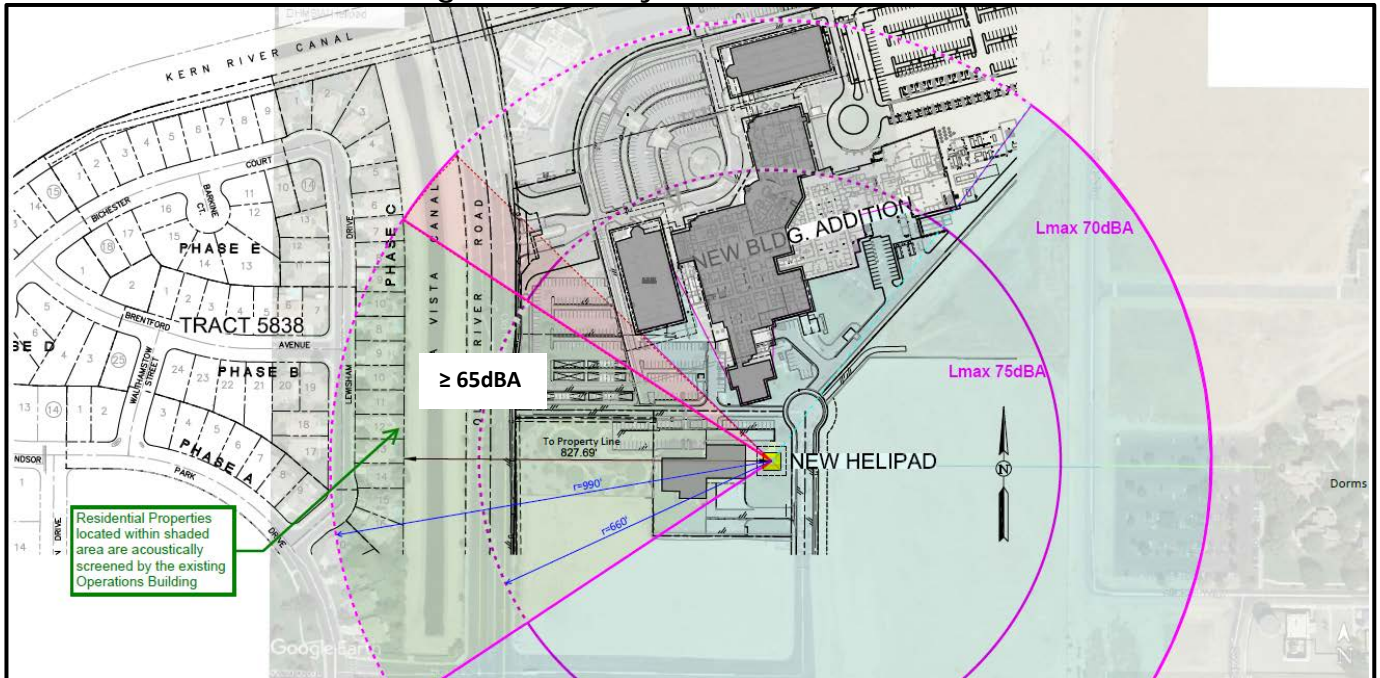
**Noise (Nearby Residential).** The nearest residential neighborhood is located approximately 800 feet to the west and California State University Bakersfield (CSUB) dormitories are located 1,000 feet to the east. A second noise study was prepared to address the noise levels for these sensitive receptors. The maximum daytime and nighttime noise levels established in the Bakersfield General Plan are 75 dBA and 70 dBA, respectively. The study determined that the predicted noise levels during helicopter landing and takeoff outside the sensitive receptors would be lower than the maximum day and nighttime noise limits. An existing operations building is located 38 feet west of the proposed helipad location. The building would provide acoustical screening and reduce exterior noise levels to 65 dBA or lower for the residences located to the east. See Figure 8.

**Figure 7. Hospital Noise Study Area**





Figure 8. Nearby Residential Levels



#### SIMILAR REQUESTS:

**Conditional Use Permit No. 4101.** On October 3, 1984, the City Council approved a conditional use permit to allow a heliport in an R-1-HOSP (One Family Dwelling-Hospital) zone located at Bakersfield Memorial Hospital on 34<sup>th</sup> Street (Resolution No. 319).

#### ENVIRONMENTAL REVIEW AND DETERMINATION:

This project has been found to be exempt from the provisions of the California Environmental Quality Act (CEQA) and the City of Bakersfield's CEQA Implementation Procedures in accordance with Section 15301, because the project consists of the operation, repair, maintenance, permitting, leasing, licensing, or minor alteration of existing public or private structures, facilities, mechanical equipment, or topographical features, involving negligible or no expansion of existing or former use. A Notice of Exemption has been prepared.

#### PUBLIC NOTIFICATION:

Public notice for the proposed project and environmental determination was advertised in *The Bakersfield Californian* and posted on the bulletin board in the City of Bakersfield Development Services Building, 1715 Chester Avenue, Bakersfield, California. All property owners within 300 feet of the project site were notified by United States mail regarding this public hearing in accordance with city ordinance and state law.

The applicant consulted with California State University Bakersfield (CSUB), the adjacent property to the east of the site. According to the applicant, CSUB is in support of the request, but they are still coordinating final documents. Staff notes that if the location of the helipad or any design feature substantially changes, the applicant will be required to re-notice the public and obtain approval of a new/revised conditional use permit.



**Public Comments Received.** In response to the public hearing notice, one written comment was received from Sonya Shea, who resides in the residential area to the west of the proposed helipad. The commenter stated that they were concerned over a noise increase in the area, and inquired whether a noise study was available.

A copy of the noise study was provided to the resident. As described in this staff report, the noise study addressing the nearby residential areas determined that noise levels would be compliant the General Plan Noise Element.

## **CONCLUSIONS:**

**Findings.** Section 17.64.060.C contains specific findings that must be made in order for your Board to approve the requested conditional use permit. Specifically, the section states that a conditional use permit shall be granted only when it is found that:

1. The proposed use is deemed essential or desirable to the public convenience or welfare; and
2. The proposed use is in harmony with the various elements and objectives of the general plan and applicable specific plans.

Section 17.64.060.D also states that a conditional use permit may be subject to such conditions as deemed appropriate or necessary to assure compliance with the intent and purpose of the zoning regulations and the various elements and objectives of the general plan and applicable specific plans and policies of the city or to protect the public health, safety, convenience, or welfare.

In accordance with these required findings, and as conditioned, Staff finds: (1) the helipad will improve emergency medical services to Bakersfield and surrounding areas; (2) the helipad is necessary to provide patients with air transport during an emergency requiring medical needs; and (3) the project would result in a development that is consistent with the intent of both the Metropolitan Bakersfield General Plan and the Bakersfield Municipal Code.

**Recommendation.** Based on the foregoing, Staff concludes the request sufficiently demonstrates compliance with the necessary findings and, therefore, recommends your Board adopt the Resolution and suggested findings approving Conditional Use Permit No. 20-0069, as depicted in the project description and subject to the listed conditions of approval.

## **ATTACHMENTS:**

Draft Resolution with Exhibits  
Public Comment  
Noise Study – Interior Noise Levels  
Noise Study – Nearby Sensitive Receptors

RESOLUTION NO. 20-\_\_\_\_\_

**RESOLUTION OF THE BAKERSFIELD BOARD OF ZONING ADJUSTMENT  
APPROVING A CONDITIONAL USE PERMIT TO ALLOW A HELIPAD  
INCIDENTAL TO AN EXISTING HOSPITAL (17.64.020.B) IN A C-2 (REGIONAL  
COMMERCIAL ZONE) DISTRICT, LOCATED AT 551 SHANLEY COURT (CUP  
NO. 20-0069).**

**WHEREAS**, McIntosh & Associates filed an application with the City of Bakersfield Development Services Department for a conditional use permit to allow a helipad incidental to an existing hospital (17.64.020.B) in a C-2 (Regional Commercial Zone) district, located at 551 Shanley Court (the "Project"); and

**WHEREAS**, the Secretary of the Board of Zoning Adjustment set Tuesday, June 9, 2020, at 3:00 p.m. in City Council Chambers, City Hall South, 1501 Truxtun Avenue, Bakersfield, California, as the time and place for a public hearing before the Board of Zoning Adjustment to consider the proposed conditional use permit, and notice of the public hearing was given in the manner provided in Title 17 of the Bakersfield Municipal Code; and

**WHEREAS**, at the public hearing (no testimony was received either in support or opposition of the Project) (testimony was received only in support/opposition/both in support and opposition of the Project); and

**WHEREAS**, the laws and regulations relating to the California Environmental Quality Act (CEQA) and the City of Bakersfield's CEQA Implementation Procedures have been duly followed by city staff and the Board of Zoning Adjustment; and

**WHEREAS**, the above described project is exempt from the requirements of CEQA in accordance with Section 15301; and

**WHEREAS**, the City of Bakersfield Development Services Department (1715 Chester Avenue, Bakersfield, California) is the custodian of all documents and other materials upon which the environmental determination is based; and

**WHEREAS**, the facts presented in the staff report and evidence received both in writing and by verbal testimony at the above referenced public hearing support the following findings:

1. All required public notices have been given. Hearing notices regarding the Project were mailed to property owners within 300 feet of the Project area and published in *The Bakersfield Californian*, a local newspaper of general circulation, 10 days prior to the hearing.
2. The provisions of CEQA, the State CEQA Guidelines, and the City of Bakersfield CEQA Implementation Procedures have been followed. Staff determined that the proposal is a project that is exempt from CEQA pursuant to Section 15301 because the project consists of the operation, repair, maintenance, permitting, leasing, licensing, or minor alteration of existing public or private structures, facilities, mechanical equipment, or topographical features, involving negligible or no expansion of existing or former use.

3. The proposed use is essential and desirable to the public convenience and welfare.
4. The proposed use is in harmony with the various elements and objectives of the Metropolitan Bakersfield General Plan.
5. The project would result in a development that is consistent with the intent of both the Metropolitan Bakersfield General Plan and the Bakersfield Municipal Code.

**NOW, THEREFORE, BE IT RESOLVED** by the Bakersfield Board of Zoning Adjustment as follows:

1. The above recitals, incorporated herein, are true and correct.
2. This project is exempt from the requirements of CEQA.
3. Conditional Use Permit No. 20-0069 as described in this resolution, is hereby approved subject to the conditions of approval in Exhibit A and as shown in Exhibits B and C.

**I HEREBY CERTIFY** that the foregoing Resolution was passed and adopted by the Board of Zoning Adjustment of the City of Bakersfield at a regular meeting held on the 9th day of June 2020, on a motion by Member \_\_\_\_\_ by the following vote:

AYES:

NOES:

RECUSE:

ABSTAIN

ABSENT:

APPROVED

\_\_\_\_\_  
STUART PATTESON, CHAIR  
City of Bakersfield Board of Zoning Adjustment

Exhibits:     A.     Conditions of Approval  
                  B.     Location Map  
                  C.     Site Plan

Exhibit A  
(Conditions of Approval)

## **EXHIBIT A**

### **CONDITIONS OF APPROVAL Conditional Use Permit No. 20-0069**

**I. The applicant's rights granted by this approval are subject to the following provisions:**

- The project shall be in accordance with all approved plans, conditions of approval, and other required permits and approvals. All construction shall comply with applicable building codes.
- All conditions imposed shall be diligently complied with at all times and all construction authorized or required shall be diligently prosecuted to completion before the premises shall be used for the purposes applied for under this approval.
- This approval will not be effective until ten (10) days after the date upon which it is granted by the BZA to allow for appeal to the City Council. Any permit or license for any approval granted shall not be issued until that effective date.
- This approval shall automatically be null and void two (2) years after the effective date unless the applicant or successor has actually commenced the rights granted, or if the rights granted are discontinued for a continuous period of one (1) year or more. This time can be extended for up to one (1) additional year by the approving body.
- The BZA may initiate revocation of the rights granted if there is good cause, including but not limited to, failure to comply with conditions of approval, complete construction or exercise the rights granted, or violation by the owner or tenant of any provision of the Bakersfield Municipal Code pertaining to the premises for which the approval was granted. The BZA may also consider adding or modifying conditions to ensure the use complies with the intent of City ordinances.
- Unless otherwise conditioned, this approval runs with the land and may continue under successive owners provided all the above mentioned provisions are satisfied.

**II. The following conditions shall be satisfied as part of the approval of this project:**

1. In consideration by the City of Bakersfield for land use entitlements, including but not limited to related environmental approvals related to or arising from this project, the applicant, and/or property owner and/or subdivider ("Applicant" herein) agrees to indemnify, defend, and hold harmless the City of Bakersfield, its officers, agents, employees, departments, commissioners and boards ("City" herein) against any and all liability, claims, actions, causes of action or demands whatsoever against them, or any of them, before administrative or judicial tribunals of any kind whatsoever, in any way arising from, the terms and provisions of this application, including without limitation any CEQA approval or any related development approvals or conditions whether imposed by the City, or not, except for CITY's sole active negligence or willful misconduct.

This indemnification condition does not prevent the Applicant from challenging any decision by the City related to this project and the obligations of this condition apply regardless of whether any other permits or entitlements are issued.

The City will promptly notify Applicant of any such claim, action or proceeding, falling under this condition within thirty (30) days of actually receiving such claim. The City, in its sole discretion, shall be allowed to choose the attorney or outside law firm to defend the City at the sole cost and expense of the Applicant and the City is not obligated to use any law firm or attorney chosen by another entity or party.

2. This conditional use permit allows for a helipad incidental to an existing hospital in the C-2 (Regional Commercial Zone) district located at 551 Shanley Court as depicted on attached Exhibits B and C.
3. No vehicle traffic will be allowed over the helipad.
4. The Hospital is responsible for securing the proposed helipad site and adjacent pedestrian sidewalks for a safe distance prior to helicopter landings and take-off.
5. Heliport facilities, may only be used as a take-off/landing pad. No maintenance, other than for emergencies, shall occur on the site.
6. Approach/departure paths shall only be from the south.
7. Existing telephone lines, parking lot light standards, and landscaping which may present a flight hazard shall be removed prior to pad construction, in accordance with appropriate state and federal guidelines.

**III. The following are specific items that you need to resolve before you can obtain a building permit or be allowed occupancy. These items include conditions and/or mitigation required by previous site entitlement approvals (these will be specifically noted), changes or additions that need to be shown on the final building plans, alert you to specific fees, and other conditions for your project to satisfy the City's development standards.**

The items listed below will usually need to be shown on the final building plans or completed before a building permit is issued. As part of the building permit submittal, identify the location of your response by using the *APPLICANT'S RESPONSE* line provided directly below the item (example: sheet number, detail, etc.).

**A. DEVELOPMENT SERVICES - BUILDING (1715 Chester Avenue)  
(Staff contact - Oscar Fuentes; 661-326-3676 or OFuentes@bakersfieldcity.us)**

1. Include with or show on the final building plans information necessary to verify that the project complies with all accessibility requirements of Title 24 of the California Building Code.

APPLICANT'S RESPONSE: \_\_\_\_\_

2. The Building Division will calculate and collect the appropriate school district impact fee at the time they issue a building permit.

APPLICANT'S RESPONSE: \_\_\_\_\_

3. Final Building plans shall show pedestrian access pathways or easements for persons with disabilities from public rights-of-ways that connect to all accessible buildings, facilities, elements, and spaces in accordance with the California Building Code. These pedestrian access ways shall not be parallel to vehicular lanes unless separated by curbs or railings.

APPLICANT'S RESPONSE: \_\_\_\_\_

**B. DEVELOPMENT SERVICES - PLANNING (1715 Chester Avenue)**

**(Staff contact – Cassandra Gale; 661-326-3411 or kgale@bakersfieldcity.us)**

1. All parking lots, driveways, drive aisles, loading areas, and other vehicular access ways, shall be paved with concrete, asphaltic concrete, or other paved street surfacing material in accordance with the Bakersfield Municipal Code (Sections 15.76.020 and 17.58.060.A.).

APPLICANT'S RESPONSE: \_\_\_\_\_

2. The developer shall meet all regulations of the San Joaquin Valley Air Pollution Control District (Regulation VIII) concerning dust suppression during construction of the project. Methods include, but are not limited to; use of water or chemical stabilizer/suppressants to control dust emission from disturbed area, stock piles, and access ways; covering or wetting materials that are transported off-site; limit construction-related speed to 15 mph on all unpaved areas/washing of construction vehicles before they enter public streets to minimize carryout/track out; and cease grading and earth moving during periods of high winds (20 mph or more).

APPLICANT'S RESPONSE: \_\_\_\_\_

3. Prior to receiving final building or site occupancy, you must contact the Planning Division (staff contact noted above) for final inspection and approval of related site improvements. Inspections will not be conducted until all required items have been installed. Any deviations from the approved plans without prior approval from the Planning Division may result in reconstruction and delays in obtaining a building or site occupancy.

APPLICANT'S RESPONSE: \_\_\_\_\_

**C. FIRE DEPARTMENT (2101 H Street)**

**(Staff contact - Ernie Medina; 661-326-3682 or EMedina@bakersfieldcity.us)**

1. Show on the final building plans the following items:
  - a. All fire hydrants, both offsite (nearest to site) and on-site. Include flow data on all hydrants. Hydrants shall be in good working condition and are subject to testing for verification. Fire flow requirements must be met prior to construction commencing on the project site. Please provide two (2) sets of the water plans stamped by a licensed Registered Civil Engineer to the Fire Department and two (2) sets to the Water Resources Department (1000 Buena Vista Road, Bakersfield, CA. 93311; 661-326-3715).

(**Note:** Show: 1) distance to the nearest hydrant; and 2) distance from that hydrant to the farthest point of the project site.)

2. The minimum gate width shall be 20 feet and a minimum vertical clearance of thirteen (13) feet six (6) inches is required when the gate is at full open position. Where there is more than one gate and two or more driveways separated by islands, a minimum horizontal clearance of fifteen (15) feet for each driveway and a minimum vertical clearance of thirteen (13) feet six (6) inches is required when gates are at full open position. Horizontal distance shall be measured perpendicular to the direction of travel on the driveway. Vertical distance shall be measured from the highest elevation of the driveway to the lowest overhead obstruction.

APPLICANT'S RESPONSE: \_\_\_\_\_

3. Electric gates shall be equipped with a means of opening the gate by fire department personnel for emergency access. Emergency opening devices shall be approved by the fire code official. All new construction of access gates shall be equipped with an automatic opening device which is activated through the wireless activation system utilized on City of Bakersfield owned vehicles for traffic preemption. Installation and maintenance of the wireless activation system on access gate(s) shall be completed by the gate owner. The gate opening device shall have a disconnect feature for manual operation of the gate when the power fails.

APPLICANT'S RESPONSE: \_\_\_\_\_

4. All projects must comply with the current California Fire Code and current City of Bakersfield Municipal Code.

APPLICANT'S RESPONSE: \_\_\_\_\_

**D. WATER RESOURCES (1000 Buena Vista Road)**

**(Staff contact - Tylor Hester; 661-326-3715 or [THester@bakersfieldcity.us](mailto:THester@bakersfieldcity.us))**

1. At this time, no applicable conditions for the proposed project. Conditions may be added if the project changes.

**E. PUBLIC WORKS - ENGINEERING (1501 Truxtun Avenue)**

**(Staff contact - Alerik Hoeh; 661-326-3581 or [ahoeh@bakersfieldcity.us](mailto:ahoeh@bakersfieldcity.us))**

**(Staff contact – Susanna Kormendi; 661-326-3997 or [SKormendi@bakersfieldcity.us](mailto:SKormendi@bakersfieldcity.us))**

1. All on-site areas required to be paved (i.e. parking lots, access drives, loading areas, etc.) shall consist of concrete, asphaltic concrete (Type B. A. C.) or other paved street material approved by the City Engineer. Pavement shall be a minimum thickness of 2 inches over 3 inches of approved base material (i.e. Class II A. B.) if concrete is used, it shall be a minimum thickness of 4 inches per Municipal Code Section 17.58.060.A. This paving standard shall be noted on the final building plans submitted to the Building Division before any building permits will be issued.

APPLICANT'S RESPONSE: \_\_\_\_\_



2. If a grading plan is required by the Building Division, building permits will not be issued until the grading plan is approved by **both** the Public Works Department and the Building Division.

APPLICANT'S RESPONSE: \_\_\_\_\_

3. All storm water generated on the project site, including the street frontage shall be retained onsite unless otherwise allowed by the Public Works Department (please contact the Public Works Department – Subdivisions at 661-326-3576).

APPLICANT'S RESPONSE: \_\_\_\_\_

4. If the project generates industrial waste, it shall be subject to the requirements of the Industrial Waste Ordinance. An industrial waste permit must be obtained from the Public Works Department before issuance of the building permit. To find out what type of waste is considered industrial, please contact the Wastewater Treatment Superintendent at 661-326-3249.

APPLICANT'S RESPONSE: \_\_\_\_\_

5. Before any building or site can be occupied, the developer must reconstruct or repair substandard off-site street improvements that front the site to adopted city standards as directed by the City Engineer. Please call the Construction Superintendent at 661-326-3049 to schedule a site inspection to find out what improvements may be required prior to submitting a grading plan. Any off-site/frontage improvements or repairs required during the site inspection shall be shown on the grading plan.

APPLICANT'S RESPONSE: \_\_\_\_\_

6. A street permit from the Public Works Department shall be obtained before any work can be done within the public right-of-way (streets, alleys, easements). Please include a copy of this site plan review decision to the department at the time you apply for this permit.

APPLICANT'S RESPONSE: \_\_\_\_\_

7. If the project is subject to the provisions of the National Pollutant Discharge Elimination System (NPDES), a "Notice of Intent" (NOI) to comply with the terms of the General Permit to Discharge Storm Water Associated with Construction Activity (SWRCB Order No. 2009-009-DWQ as amended by Order No. 2010-0014-DWQ and 2012-0006-DWQ) must be filed with the State Water Resources Control Board in Sacramento before the beginning of any construction activity. Compliance with the general permit required that a Storm Water Pollution Prevention Plan (SWPPP) be prepared, continuously carried out, and always be available for public inspection during normal construction hours.

APPLICANT'S RESPONSE: \_\_\_\_\_

8. Prior to the issuance of each building permit, or if no building permit is required, the first required City approval prior to construction, the developer/owner shall pay a Transportation Impact Fee (TIF) for regional facilities. This fee will be based on the rate in effect at the time the applicable approval is issued or in accordance with the Subdivision Map Act, as applicable. The Public Works Department will calculate an estimate of the total fee upon submittal of construction plans for the project.

APPLICANT'S RESPONSE: \_\_\_\_\_

9. The developer shall form a new Maintenance District. Undeveloped parcels within an existing Maintenance District are required to update Maintenance District documents. Updated documents, including Proposition 218 Ballot and Covenant, shall be signed and notarized. If there are questions, contact Alerik Hoeh at 661-326-3576.

(**Note:** If already within a maintenance district, may need to update the maintenance district form.)

APPLICANT'S RESPONSE: \_\_\_\_\_

10. This project may be located within a Planned Drainage Area. Please contact the Public Works Department – Subdivisions at 661-326-3576 to determine what fees may apply.

APPLICANT'S RESPONSE: \_\_\_\_\_

11. This project may be subject to Bridge and Major Thoroughfare fees. Please contact the Public Works Department – Subdivisions at 661-326-3576 to determine what fees may apply.

APPLICANT'S RESPONSE: \_\_\_\_\_

**F. PUBLIC WORKS - TRAFFIC (1501 Truxtun Avenue)**

**(Staff contact - Alerik Hoeh; 661-326-3581 or ahoeh@bakersfieldcity.us)**

**(Staff contact – Susanna Kormendi; 661-326-3997 or SKormendi@bakersfieldcity.us)**

1. At this time, no applicable conditions for the proposed project. Conditions may be added if the project changes.

**G. PUBLIC WORKS - SOLID WASTE (4101 Truxtun Avenue)**

**(Staff Contact - Jesus Carrera; 661-326-3114 or jcarrera@bakersfieldcity.us)**

1. At this time, no applicable conditions for the proposed project. Conditions may be added if the project changes.

**F. PUBLIC WORKS - TRAFFIC (1501 Truxtun Avenue)**

**(Staff contact - Alerik Hoeh; 661-326-3581 or ahoeh@bakersfieldcity.us)**

**(Staff contact – Susanna Kormendi; 661-326-3997 or SKormendi@bakersfieldcity.us)**

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**G. PUBLIC WORKS - SOLID WASTE (4101 Truxtun Avenue)**

**(Staff Contact - Jesus Carrera; 661-326-3114 or jcarrera@bakersfieldcity.us)**

1. At this time, no applicable conditions for the proposed project. Conditions may be added if the project changes.

Exhibit B  
(Location Map)

CUP 20-0069

CITY OF BAKERSFIELD

LEGEND  
(ZONE DISTRICTS)

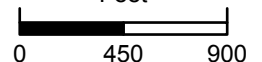
- R-1 One Family Dwelling  
6,000 sq.ft. min lot size
- R-1.4.5 One Family Dwelling  
4,500 sq.ft. min lot size
- E Estate  
10,000 sq.ft. min lot size
- R-S Residential Suburban  
24,000 sq.ft./dwelling unit
- R-S( ) Residential Suburban  
1, 2.5, 5 or 10 min lot size
- R-2 Limited Multiple Family Dwelling  
4,500 sq.ft. min lot size (single family)  
6,000 sq.ft. min lot size (multifamily)  
2,500 sq.ft. lot area/dwelling unit
- R-3 Multiple Family Dwelling  
6,000 sq.ft. min lot size  
1,250 sq.ft. lot area/dwelling unit
- R-4 High Density Multiple Family Dwelling  
6,000 sq.ft. min lot size  
600 sq.ft. lot area/dwelling unit
- R-H Residential Holding  
20 acre min lot size
- A Agriculture  
6,000 sq.ft. min lot size
- A-20A Agriculture  
20 acre min lot size
- PUD Planned Unit Development
- TT Travel Trailer Park
- MH Mobilehome
- C-O Professional and Administrative Office
- C-1 Neighborhood Commercial
- C-2 Regional Commercial
- C-C Commercial Center
- C-B Central Business
- PCD Planned Commercial Development
- M-1 Light Manufacturing
- M-2 General Manufacturing
- M-3 Heavy Industrial
- P Automobile Parking
- RE Recreation
- Ch Church Overlay
- OS Open Space
- HOSP Hospital Overlay
- AD Architectural Design Overlay
- FP-P Floodplain Primary
- FP-S Floodplain Secondary
- AA Airport Approach
- DI Drilling Island
- PE Petroleum Extraction Combining
- SC Senior Citizen Overlay
- HD Hillside Development Combining
- WM- West Ming Specific Plan



BAKERSFIELD



Feet



Document Name: 2020\_02\_02

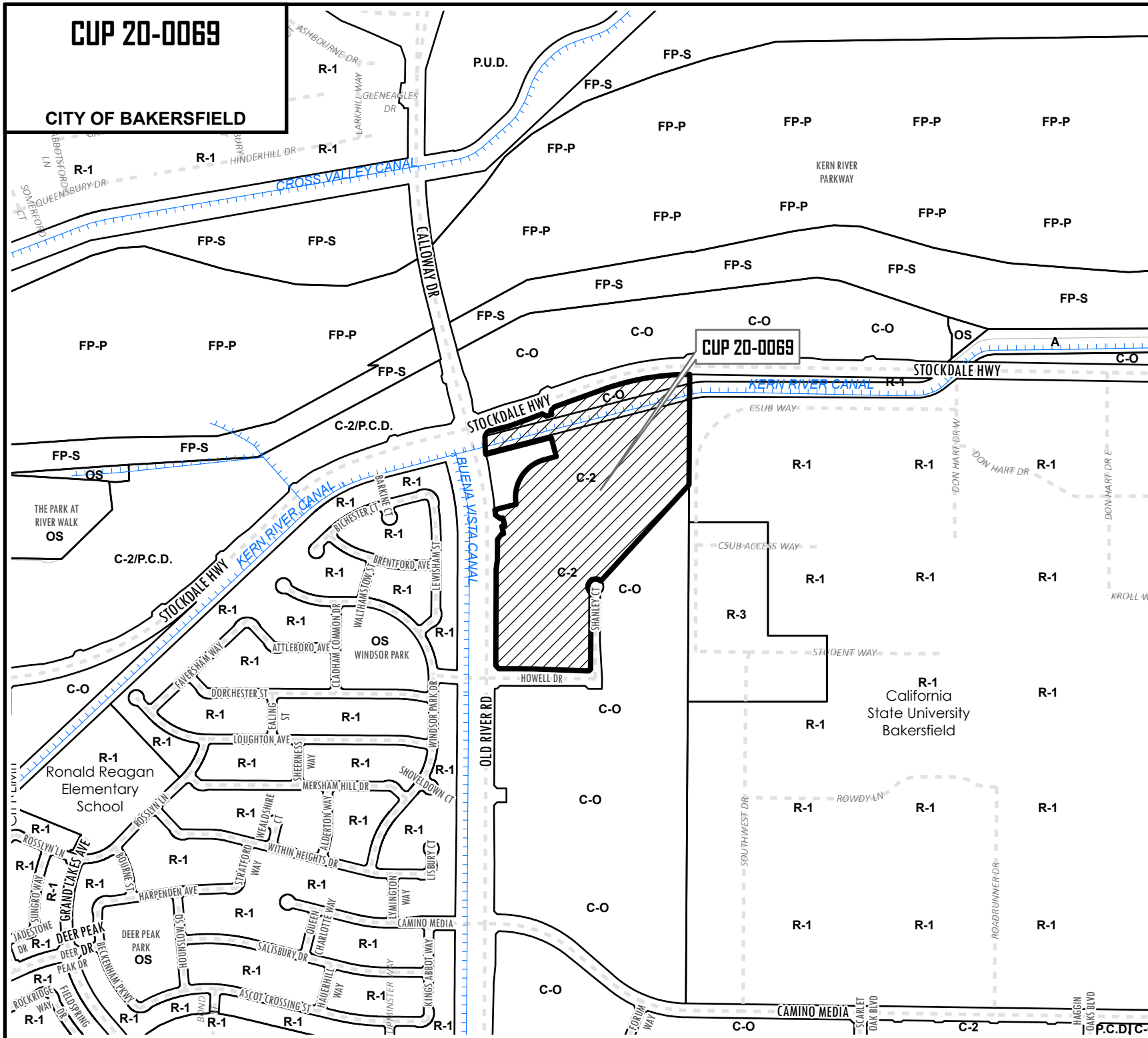
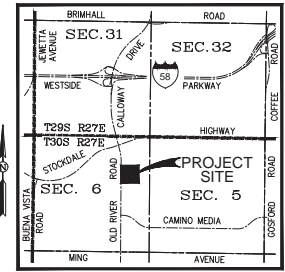
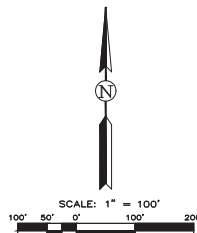
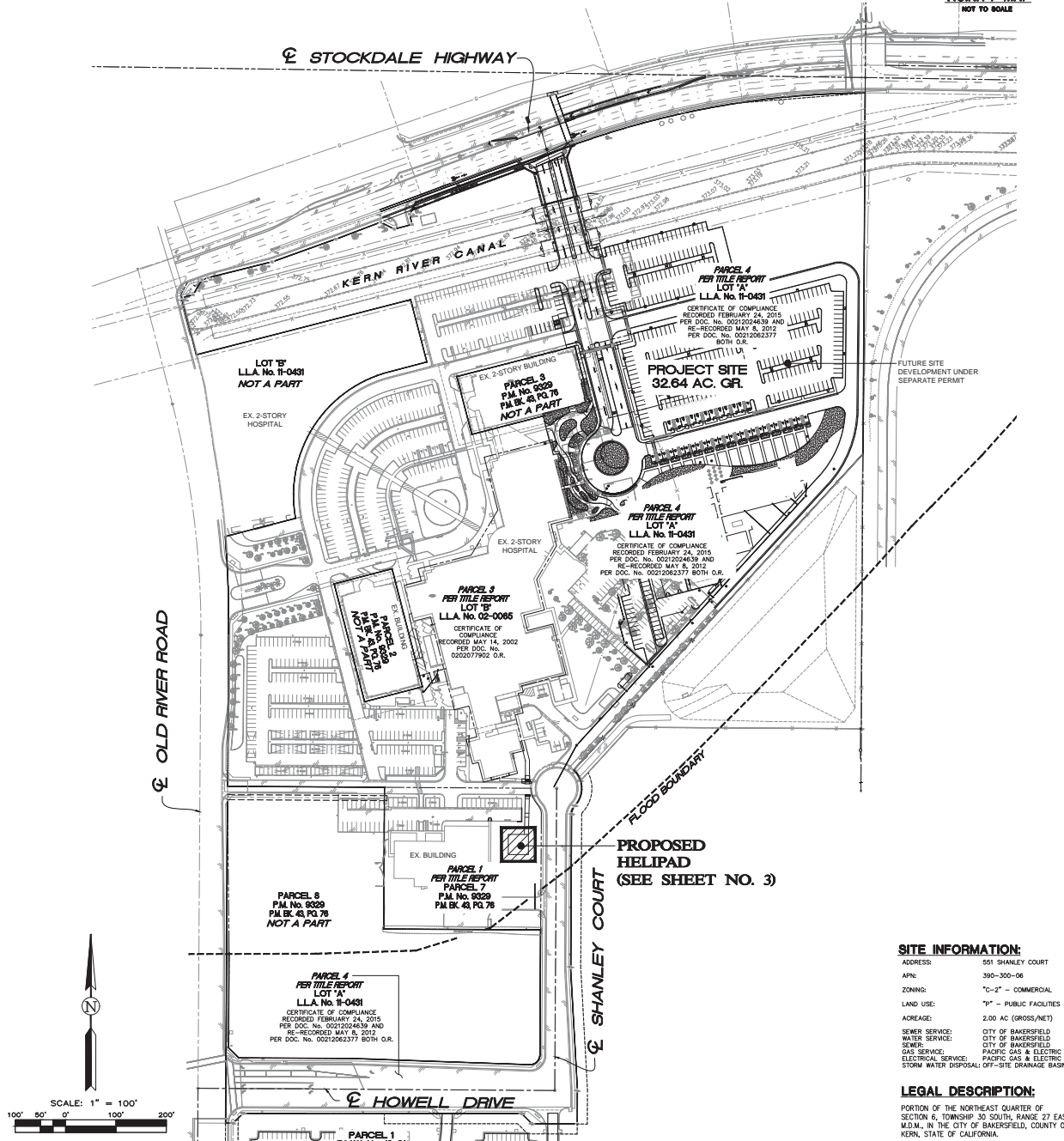


Exhibit C  
(Site Development Plan)

CITY OF BAKERSFIELD  
**CONDITIONAL USE PERMIT**  
 PROPOSED HELIPAD  
 DIGNITY HEALTH-MERCY SOUTHWEST HOSPITAL  
 551 SHANLEY COURT  
 BAKERSFIELD, CA 93311



VICINITY MAP  
 NOT TO SCALE



**TOPOGRAPHY LEGEND:**

- |                                       |  |
|---------------------------------------|--|
| EXISTING ELECTRICAL BOX               | EXISTING OIL PIPELINE                    |
| EXISTING TRAFFIC SIGN                 | EXISTING SENIOR LINE                     |
| EXISTING STREET NAME SIGN             | EXISTING STORM DRAIN LINE                |
| EXISTING IRRIGATION VALVE OR RISER    | EXISTING WATER LINE                      |
| EXISTING WATER VALVE                  | EXISTING TELEPHONE LINE - APPROX.        |
| EXISTING WATER METER                  | EXISTING GAS LINE - APPROX.              |
| EXISTING FIRE HYDRANT                 | EXISTING U.S. ELECTRICAL LINES - APPROX. |
| EXISTING SENIOR MANHOLE               | EXISTING CHAIN LINK FENCE                |
| EXISTING SENIOR CLEANOUT              | EXISTING CONTOUR LINE                    |
| EXISTING STORM DRAIN MANHOLE          | EXISTING CURB AND GUTTER                 |
| EXISTING GAS VALVE                    |  |
| EXISTING LIGHT STANDARD ON CONC. BASE |  |
| EXISTING STREET LIGHT                 |  |
| EXISTING STREET LIGHT PULL BOX        |  |

**KEY MAP**  
 SCALE: 1" = 100'

**DEVELOPMENT NOTES:**

1. THERE ARE EXISTING STRUCTURES ON-SITE.
2. NO DEMOLITION PROPOSED.
3. NO LANDSCAPE REQUIREMENTS ARE ANTICIPATED.
4. NO SITE LIGHTING PROPOSED.
5. NO PARKING MODIFICATIONS ARE ANTICIPATED.
6. NO VERTICAL STRUCTURES PROPOSED.

**SHEET INDEX:**

- SH 1 - COVER SHEET
- SH 2 - BOUNDARY & EASEMENT MAP
- SH 3 - SITE DEVELOPMENT PLAN

**SITE INFORMATION:**

ADDRESS: 551 SHANLEY COURT  
 APT: 390-305-06  
 ZONING: "C-2" - COMMERCIAL  
 LAND USE: "P" - PUBLIC FACILITIES  
 ACREAGE: 2.00 AC (GROSS/NET)  
 SEWER SERVICE: CITY OF BAKERSFIELD  
 WATER SERVICE: CITY OF BAKERSFIELD  
 GAS SERVICE: PACIFIC GAS & ELECTRIC  
 ELECTRICAL SERVICE: PACIFIC GAS & ELECTRIC  
 STORM WATER DISPOSAL: OFF-SITE DRAINAGE BASIN

**LEGAL DESCRIPTION:**

PORTION OF THE NORTHEAST QUARTER OF SECTION 6, TOWNSHIP 30 SOUTH, RANGE 27 EAST, M.D.M., IN THE CITY OF BAKERSFIELD, COUNTY OF KERN, STATE OF CALIFORNIA.

**OWNER/DEVELOPER**

DIGNITY HEALTH-MERCY HOSPITAL SOUTHWEST  
 2215 TRUKSTON AVENUE  
 BAKERSFIELD, CA 93301  
 CONTACT PERSON: BRUCE PETERS  
 PHONE: (661) 632-5537

**ENGINEER**

MCINTOSH & ASSOCIATES  
 2001 WHEELAN COURT  
 BAKERSFIELD, CA 93309  
 CONTACT: ROGER A. MCINTOSH  
 PHONE: (661) 834-4814

**NOTES:**

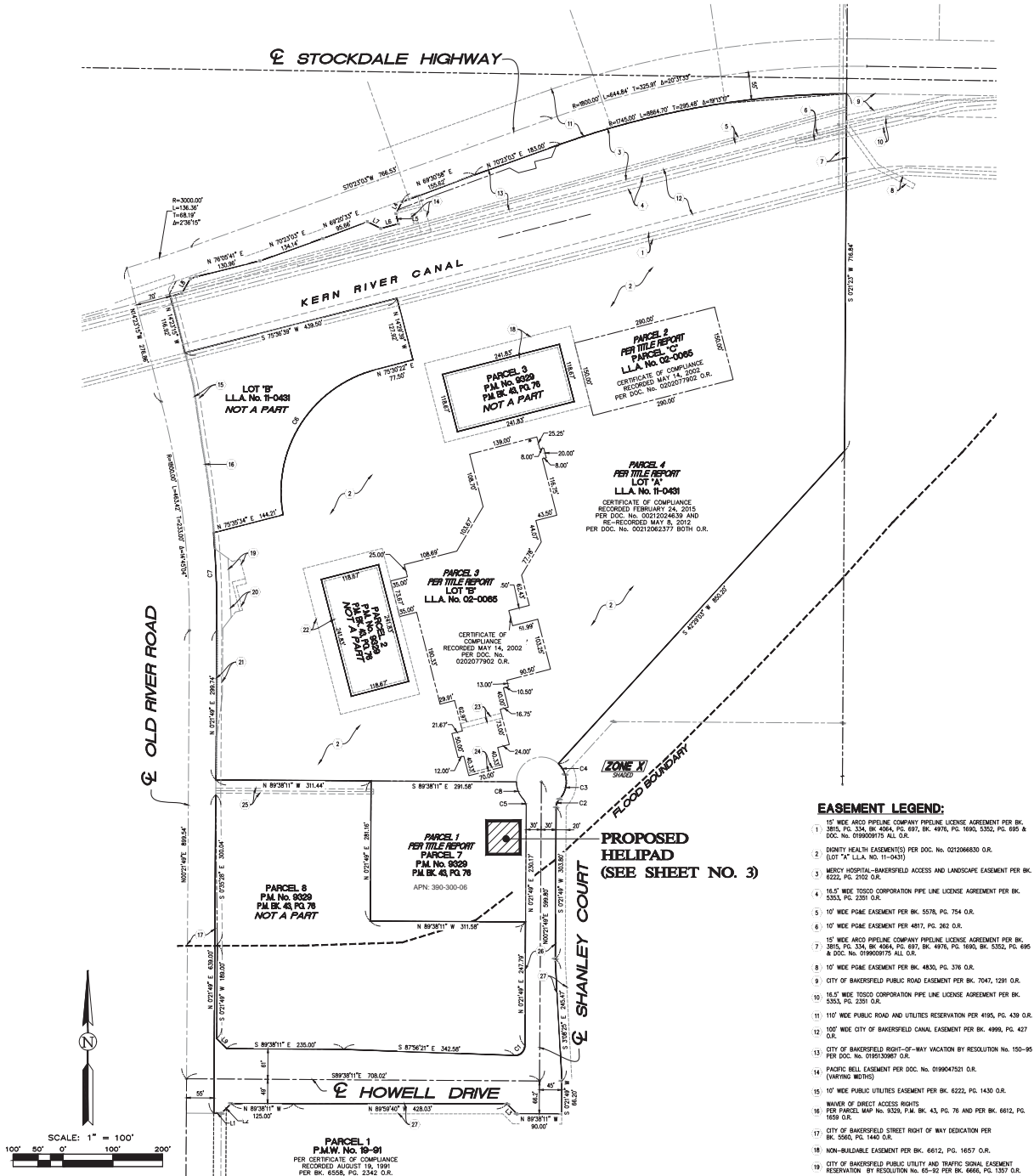
NOT ALL UNDERGROUND UTILITIES OR FACILITIES WERE LOCATED BY THIS SURVEY AND THE SURVEYOR ASSUMES NO RESPONSIBILITY FOR UNDERGROUND UTILITIES NOT SHOWN OR FOR INFORMATION DERIVED FROM ANY OUTSIDE SOURCES.

SHEET <b>1</b> OF 8 SHEETS	DIGNITY HEALTH - MERCY SOUTHWEST HOSPITAL 400 OLD RIVER ROAD, BAKERSFIELD, CA 93311 <b>CONDITIONAL USE PERMIT</b> COVER SHEET	DATE	REVISIONS	BY	2001 WHEELAN COURT BAKERSFIELD, CALIFORNIA 93309 (661) 834 - 4814 NAME: Mr. Chappas DESIGNED FOR: REVIEW DATE: 3/9/20	
		DATE	REVISIONS	BY		

# CITY OF BAKERSFIELD CONDITIONAL USE PERMIT

PROPOSED HELIPAD

DIGNITY HEALTH-MERCY SOUTHWEST HOSPITAL  
551 SHANLEY COURT  
BAKERSFIELD, CA 93311



CURVE TABLE				
CURVE	RADIUS	LENGTH	TANGENT	DELTA
C1	20.00'	32.01'	20.00'	91°41'50"
C2	25.00'	18.89'	9.81'	42°50'00"
C3	50.00'	52.86'	29.28'	60°42'29"
C4	50.00'	26.18'	13.40'	30°00'17"
C5	25.00'	18.89'	9.81'	42°50'00"
C6	253.00'	378.65'	235.83'	85°58'38"
C7	1860.00'	163.67'	81.89'	210°20'30"
C8	50.00'	37.38'	19.61'	42°50'00"

LINE TABLE		
LINE	BEARING	DISTANCE
L1	N 89°38'11" W	15.00'
L2	S 45°21'48" W	28.28'
L3	N 44°48'56" W	28.20'
L4	N 27°33'50" E	26.71'
L5	N 14°23'15" W	21.00'
L6	N 75°36'38" E	37.00'
L7	S 62°31'21" E	28.79'
L8	N 30°37'20" E	34.18'
L9	S 44°38'11" E	28.28'

SHEET  
**2**  
OF 8 SHEETS

DIGNITY HEALTH - MERCY SOUTHWEST HOSPITAL  
400 OLD RIVER ROAD, BAKERSFIELD, CA 93311  
**CONDITIONAL USE PERMIT**  
**BOUNDARY MAP**

DATE	REVISIONS	BY



2001 WHEELAN COURT  
BAKERSFIELD, CALIFORNIA 93309  
(661) 834 - 4814  
OWNER: City of Bakersfield  
DATE: 3/9/20  
DESIGNED BY: [Signature]  
CHECKED BY: [Signature]  
DATE: 3/9/20

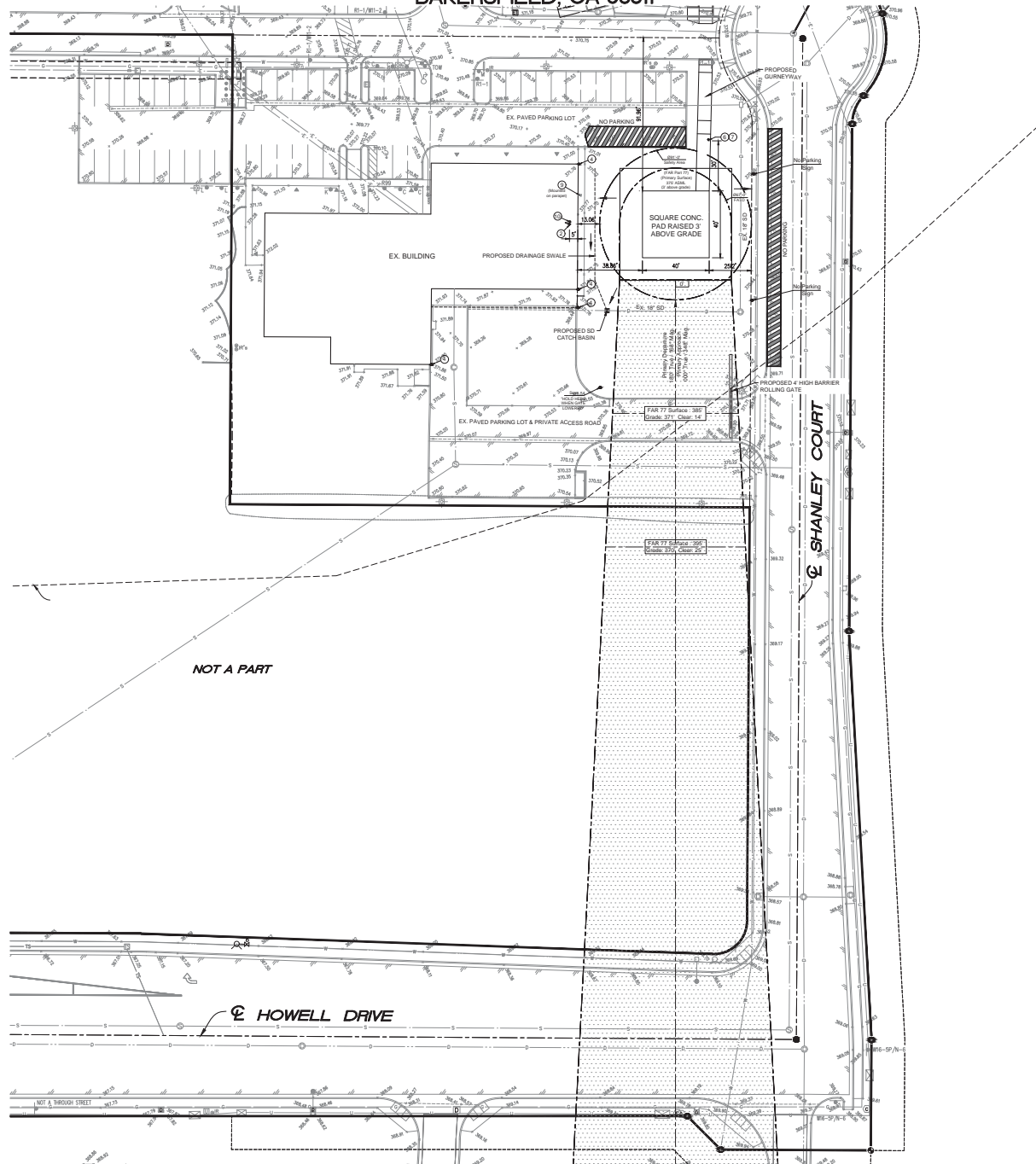
**McINTOSH & ASSOCIATES**  
LAND SURVEYING • CIVIL ENGINEERING © 2020



# CITY OF BAKERSFIELD CONDITIONAL USE PERMIT

## PROPOSED HELIPAD

DIGNITY HEALTH-MERCY SOUTHWEST HOSPITAL  
551 SHANLEY COURT  
BAKERSFIELD, CA 93311



### TOPOGRAPHY LEGEND:

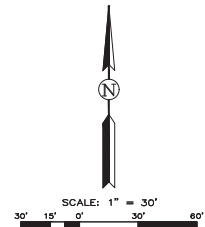
EXISTING ELECTRICAL BOX	EXISTING OIL PIPELINE
EXISTING TRAFFIC SIGN	EXISTING SEWER LINE
EXISTING STREET NAME SIGN	EXISTING STORM DRAIN LINE
EXISTING IRRIGATION VALVE OR RISER	EXISTING WATER LINE
EXISTING WATER VALVE	EXISTING TELEPHONE LINE - APPROX.
EXISTING WATER METER	EXISTING GAS LINE - APPROX.
EXISTING FIRE HYDRANT	EXISTING U.S. ELECTRICAL LINE(S) - APPROX.
EXISTING SEWER MANHOLE	EXISTING CHAIN LINK FENCE
EXISTING SEWER CLEANOUT	EXISTING CONTOUR LINE
EXISTING STORM DRAIN MANHOLE	EXISTING CURB AND GUTTER
EXISTING GAS VALVE	
EXISTING LIGHT STANDARD ON CONC. BASE	
EXISTING STREET LIGHT	
EXISTING STREET LIGHT PULL BOX	

### DEVELOPMENT LEGEND:

- (1) PERIMETER LIGHTS (8)
- (2) LIGHTED WIND INDICATOR (1)
- (3) W/F RADIO CONTROLLER (1)
- (4) OBSTRUCTION LIGHTS (4)
- (5) FLOOD LIGHTS (8)
- (6) NO SMOKING / CAUTION SIGN (1)
- (7) FIRE EXTINGUISHER (1)
- (8) POLICES / PROCEDURES SIGN (1)
- (9) RECORDING CCTV (1)
- (10) WEATHER SYSTEM (1)

### NOTES:

1. HELIPORT AND GURNEYWAY MEDIUM BROOM FINISHED CONCRETE. FULL GURNEYWAY JOINTS WITH SEALING COMPOUND OR THIN-SLICE CONCRETE TO PROVIDE SMOOTH GURNEY OPERATIONS.
2. 8" DIAMETER SAFETY AREA TO REMAIN CLEAR OF ALL ITEMS MORE THAN 2' ABOVE THE LEVEL OF THE HELIPORT



SHEET  
3

OF 8 SHEETS

DIGNITY HEALTH - MERCY SOUTHWEST HOSPITAL  
400 OLD RIVER ROAD, BAKERSFIELD, CA 93311  
CONDITIONAL USE PERMIT  
SITE DEVELOPMENT PLAN

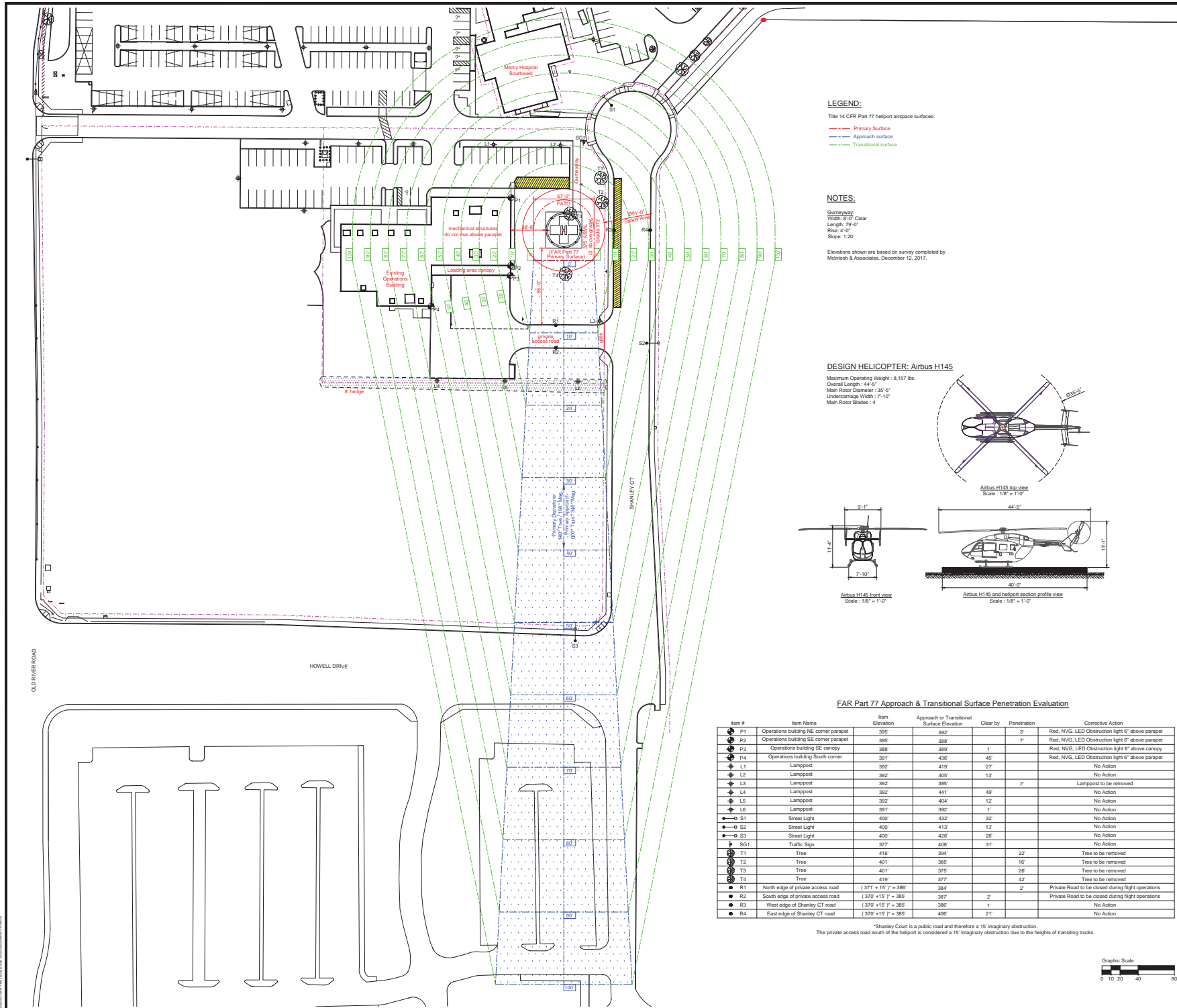
ADWACAD FILE: 1701010102

DATE	REVISIONS	BY



2001 WHEELAN COURT BAKERSFIELD, CALIFORNIA 93309 (661) 834 - 4814
DATE: 3/9/20
REVIEW: 3/9/20

**McINTOSH & ASSOCIATES**  
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LEGEND:

Title 14 CFR Part 77 helipad airspace surfaces:  
Primary Surface  
Approach surface  
Transitional surface

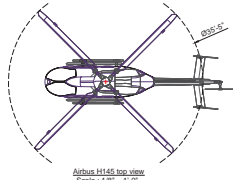
NOTES:

Curvature:  
Width: 8'-0" Clear  
Length: 75'-0"  
Rise: 4'-0"  
Slope: 1:20

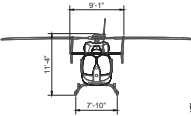
Elevations shown are based on survey completed by  
McIntosh & Associates, December 12, 2017.

DESIGN HELICOPTER: Airbus H145

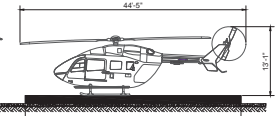
Maximum Operating Weight: 6,157 lbs.  
Overall Length: 44'-0"  
Main Rotor Diameter: 35'-0"  
Undercarriage Width: 7'-10"  
Main Rotor Blades: 4



Airbus H145 top view  
Scale: 1/8" = 1'-0"



Airbus H145 front view  
Scale: 1/8" = 1'-0"

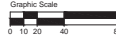


Airbus H145 side profile view  
Scale: 1/8" = 1'-0"

FAR Part 77 Approach & Transitional Surface Penetration Evaluation

Item #	Item Name	Item Elevation	Approach or Transitional Surface Elevation	Clear by	Penetration	Corrective Action
P1	Operations building NE corner parapet	392'	392'	3'		Ret. NVG, LED Obstruction light 6' above parapet
P2	Operations building SE corner parapet	395'	388'	7'		Ret. NVG, LED Obstruction light 6' above parapet
P3	Operations building SE canopy	388'	389'	1'		Ret. NVG, LED Obstruction light 6' above canopy
P4	Operations building South corner	391'	436'	45'		Ret. NVG, LED Obstruction light 6' above parapet
L1	Lamppost	392'	419'	27'		No Action
L2	Lamppost	392'	405'	13'		No Action
L3	Lamppost	392'	385'	7'		Lamppost to be removed
L4	Lamppost	392'	441'	49'		No Action
L5	Lamppost	392'	404'	12'		No Action
L6	Lamppost	391'	392'	1'		No Action
S1	Street Light	400'	432'	32'		No Action
S2	Street Light	400'	413'	13'		No Action
S3	Street Light	400'	426'	26'		No Action
SG1	Traffic Sign	377'	406'	31'		No Action
T1	Tree	416'	394'	22'		Tree to be removed
T2	Tree	401'	385'	16'		Tree to be removed
T3	Tree	401'	375'	26'		Tree to be removed
T4	Tree	419'	377'	42'		Tree to be removed
R1	North edge of private access road	( 371' + 15' ) = 386'	384'	2'		Private Road to be closed during flight operations
R2	South edge of private access road	( 370' + 15' ) = 385'	387'	2'		Private Road to be closed during flight operations
R3	West edge of Shanley CT road	( 370' + 15' ) = 385'	386'	1'		No Action
R4	East edge of Shanley CT road	( 370' + 15' ) = 385'	406'	21'		No Action

Shanley Court is a public road and therefore a 15' imaginary obstruction due to the heights of trailing trucks.  
The private access road south of the helipad is considered a 15' imaginary obstruction due to the heights of trailing trucks.



AIRSAFE  
13230 North Chiricahua Peak Drive,  
ORO VALLEY, AZ 85755



HELIPAD  
LOCATION / ELEVATION

Latitude : 35 deg 21 min. 00 sec. N  
Longitude : 119 deg. 06 min. 44 sec. W  
Elevation : 375 AMSL

AGENCY APPROVAL

OSHPD No. 119095-15-02 OSHPD ACD No.:  
Professional Stamps

Revisions	No.	Date	Description
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Document Phase: CD  
Date: 03/01/2020  
PEL/ACD:  
USA Project No.: PR16-0324

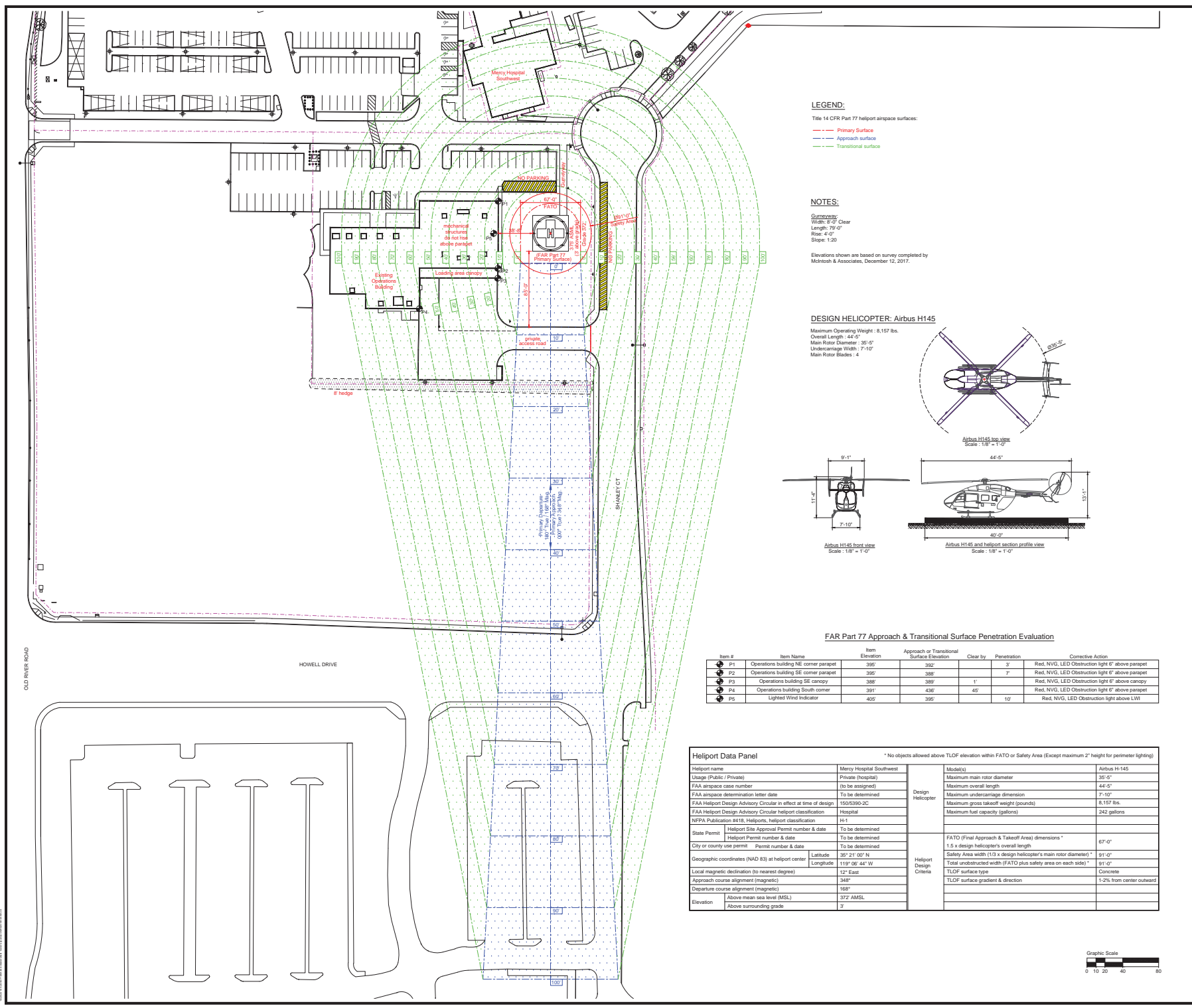
Project Title  
MERCY HOSPITAL SOUTHWEST



Sheet Title  
EXISTING/PLANNED CONDITIONS

Sheet Number

H101



LEGEND:

- Title 14 CFR Part 77 heliport airspace surfaces:
- Primary Surface
  - Approach surface
  - Transitional surface

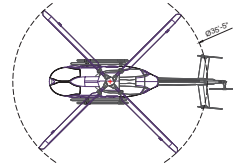
NOTES:

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Length: 75'-0"  
Rise: 4'-0"  
Slope: 1:20

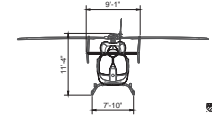
Elevations shown are based on survey completed by  
Mintosh & Associates, December 12, 2017.

DESIGN HELICOPTER: Airbus H145

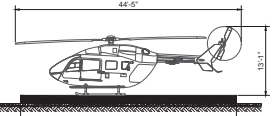
Maximum Operating Weight: 8,157 lbs.  
Overall Length: 44'-5"  
Main Rotor Diameter: 35'-5"  
Undercarriage Width: 7'-10"  
Main Rotor Blades: 4



Airbus H145 top view  
Scale: 1/8" = 1'-0"



Airbus H145 front view  
Scale: 1/8" = 1'-0"



Airbus H145 side view  
Scale: 1/8" = 1'-0"

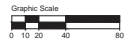
FAR Part 77 Approach & Transitional Surface Penetration Evaluation

Item #	Item Name	Item Elevation	Approach or Transitional Surface Elevation	Clear by	Penetration	Corrective Action
P1	Operations building NE corner parapet	386'	392'		3'	Red, NVG, LED Obstruction light 6" above parapet
P2	Operations building SE corner parapet	395'	395'		7'	Red, NVG, LED Obstruction light 6" above parapet
P3	Operations building SE canopy	388'	388'	1'		Red, NVG, LED Obstruction light 6" above canopy
P4	Operations building South corner	391'	436'	45'		Red, NVG, LED Obstruction light 6" above parapet
P5	Lighted Wind Indicator	405'	395'		10'	Red, NVG, LED Obstruction light above LWI

Helipad Data Panel

\* No objects allowed above TLOF elevation within FATO or Safety Area (Except maximum 2' height for perimeter lighting)

Helipad name	Mercy Hospital Southwest	Model(s)	Airbus H-145
Usage (Public / Private)	Private (hospital)	Maximum main rotor diameter	35'-5"
FAA airspace case number	(to be assigned)	Maximum overall length	44'-5"
FAA airspace determination letter date	To be determined	Maximum undercarriage dimension	7'-10"
FAA Helipad Design Advisory Circular in effect at time of design	1505390-2C	Maximum gross takeoff weight (pounds)	8,157 lbs.
FAA Helipad Design Advisory Circular helipad classification	H-1	Maximum fuel capacity (gallons)	242 gallons
NFPA Publication #418, Helipads, helipad classification	H-1		
State Permit	Helipad Site Approval Permit number & date	To be determined	
City or county use permit	Permit number & date	To be determined	
Geographic coordinates (NAD 83) at helipad center	Latitude	30° 21' 00" N	FATO (Final Approach & Takeoff Area) dimensions *
	Longitude	119° 06' 44" W	1.5 x design helicopter's overall length
Local magnetic declination (to nearest degree)	12° East		Safety Area width (1/3 x design helicopter's main rotor diameter) *
Approach course alignment (magnetic)	348°		Total unobstructed width (FATO plus safety area on each side) *
Departure course alignment (magnetic)	168°		TLOF surface type
Elevation	Above mean sea level (MSL)	372' AMSL	Concrete
	Above surrounding grade	3'	TLOF surface gradient & direction
			1-2% from center outward



Keynotes

AIRSAFE  
13230 North Chiricahua Peak Drive,  
ORO VALLEY, AZ 85755



HELIPAD LOCATION / ELEVATION

Latitude : 35 deg 21 min 00 sec N  
Longitude : 119 deg 06 min 44 sec W  
Elevation : 375 AMSL

AGENCY APPROVAL

OSHPD No. 190905-15-02  
Professional Stamps

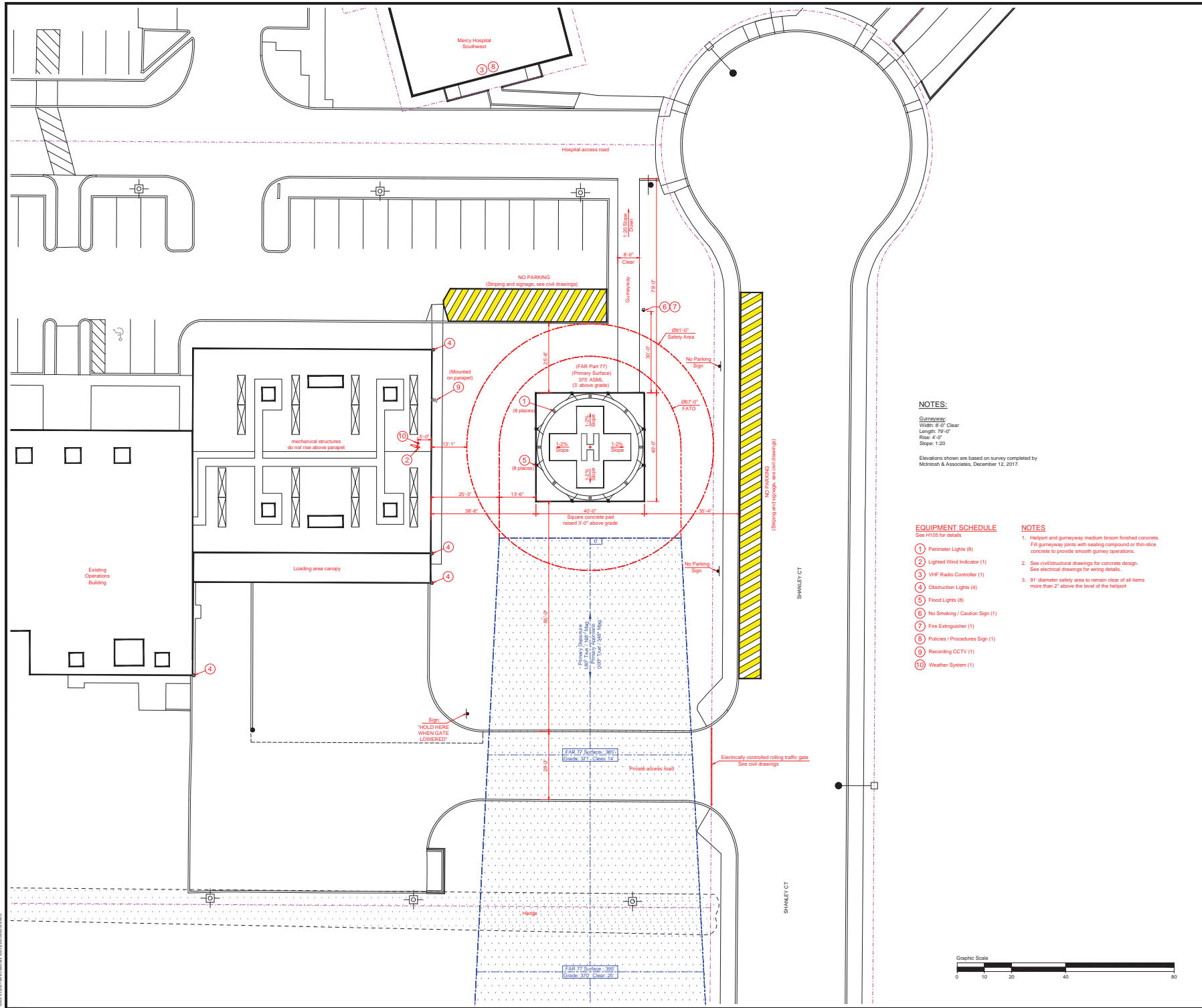
Revisions  
No. Date Description

Document Phase: CD  
Date: 03/01/2020  
P/E: JAC  
C/A: JAC  
GSA Project No.: P1915-0324

Project Title  
MERCY HOSPITAL SOUTHWEST

Dignity Health  
Sheet Title  
GENERAL CONDITION AIRSPACE  
AS-BUILT

Sheet Number  
H102



**NOTES:**

Runway:  
Width: 9'-0" Clear  
Length: 79'-0"  
Rise: 4'-0"  
Slope: 1:20

Elevations shown are based on survey completed by  
McIntosh & Associates, December 12, 2017.

**EQUIPMENT SCHEDULE**

See H105 for details.

- 1 Perimeter Lights (8)
- 2 Lighted Wind Indicator (1)
- 3 VHF Radio Controller (1)
- 4 Obstruction Lights (4)
- 5 Flood Lights (8)
- 6 No Smoking / Caution Sign (1)
- 7 Fire Extinguisher (1)
- 8 Policies / Procedures Sign (1)
- 9 Recording CCTV (1)
- 10 Weather System (1)

**NOTES:**

- 1. Heliport and gateway medium broom finished concrete. Fill gateway joints with sealing compound or thin-slice concrete to provide smooth gateway operations.
- 2. See civil/structural drawings for concrete design. See electrical drawings for wiring details.
- 3. 91' diameter safety area to remain clear of all items more than 2" above the level of the heliport.

Keynotes

**AIRSAFE**

13230 North Chiricahua Peak Drive,  
ORO VALLEY, AZ 85755



**HELIPAD LOCATION / ELEVATION**

Latitude : 35 deg 21 min. 00 sec. N  
Longitude : 119 deg. 06 min. 44 sec. W  
Elevation : 375 AMSL

**AGENCY APPROVAL**

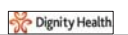
OSHPD No. 1190995-15-02  
Professional Stamps

OSHPD ACD No.:

Revisions	No.	Date	Description
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Document Phase:	CD
Date:	03/01/2018
PEI / ACD:	
USA Project No.:	PR16-0324

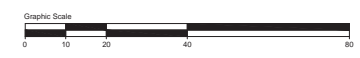
Project Title  
**MERCY HOSPITAL SOUTHWEST**



Sheet Title  
**HELIPORT FINISHED CONDITION  
AS-BUILT**

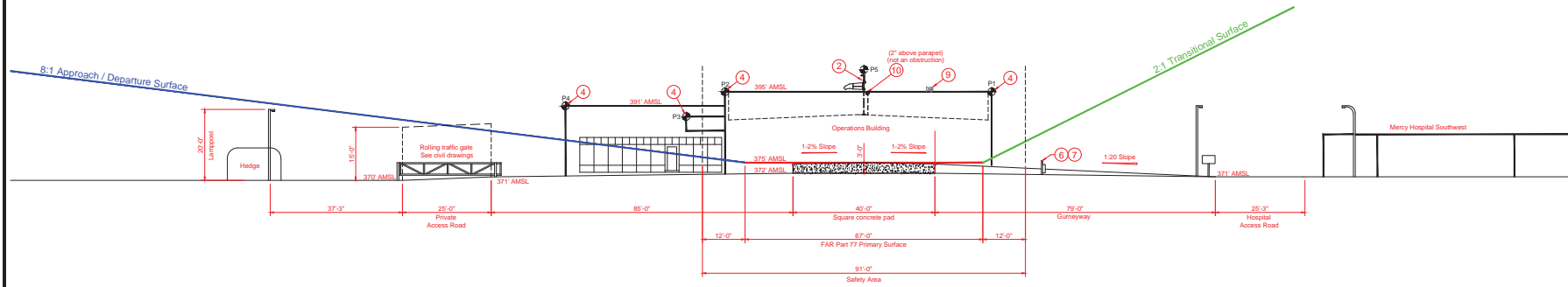
Sheet Number

**H103**

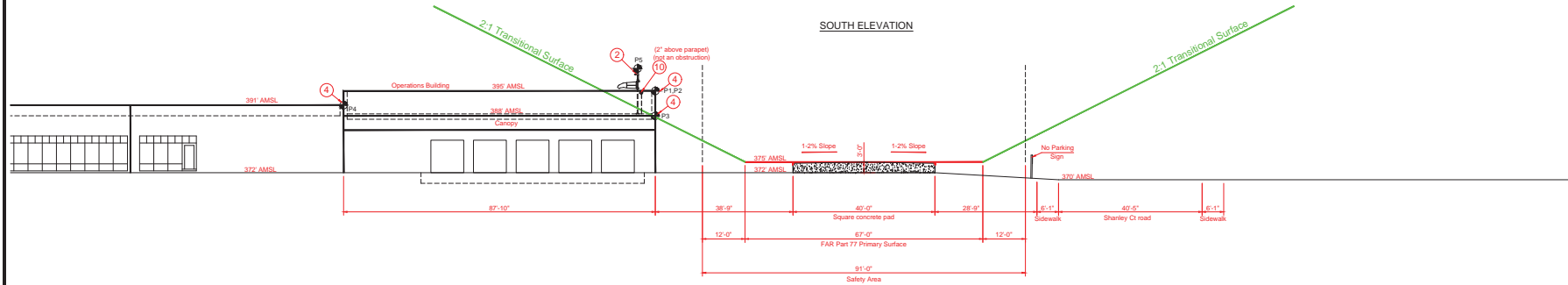


Keynotes

## WEST ELEVATION



## SOUTH ELEVATION



## FAR Part 77 Approach & Transitional Surface Penetration Evaluation

Item #	Item Name	Item Elevation	Approach or Transitional Surface Elevation	Clear by	Penetration	Corrective Action
P1	Operations building NE corner parapet	395'	392'	3'	Red, NVG, LED Obstruction light 6' above parapet	
P2	Operations building SE corner parapet	395'	388'	7'	Red, NVG, LED Obstruction light 6' above parapet	
P3	Operations building SE canopy	385'	389'	4'	Red, NVG, LED Obstruction light 6' above canopy	
P4	Operations building South corner	391'	436'	45'	Red, NVG, LED Obstruction light 6' above parapet	
P5	Lighted Wind Indicator	401'	390'	11'	Red, NVG, LED Obstruction light above LWI	

**AIRSAFE**  
 13230 North Chiricahua Peak Drive,  
 ORO VALLEY, AZ 85755

## HELIPAD LOCATION / ELEVATION

Latitude : 35 deg 21 min 00 sec N  
 Longitude : 119 deg 06 min 44 sec W  
 Elevation : 375 AMSL

## AGENCY APPROVAL

OSHPD No.1190095-15-02

OSHPD ACD No.:

Professional Stamps

Revisions  
 No. Date Description

Document Phase: CD

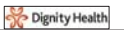
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PEI/ACD:

USA Project No.: PR15-024

Project Title

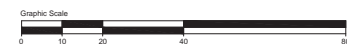
MERCY HOSPITAL SOUTHWEST



Sheet Title  
 WEST & SOUTH ELEVATIONS  
 AS-BUILT

Sheet Number

H104



-----Original Message-----

From: Sonya Shea <[sonyashea@att.net](mailto:sonyashea@att.net)>

Sent: Tuesday, May 05, 2020 1:29 PM

To: Whitney M. Jackson <[wjackson@bakersfieldcity.us](mailto:wjackson@bakersfieldcity.us)>

Subject: helipad - Mercy Southwest Hospital

Warning: This email originated from outside the City of Bakersfield. Think before you click!

Dear ms/mr/mrs Jackson:

Since I am unable to attend the meeting because of the public health official distancing guidelines, I would like to register my concern over the possibility of a severe noise increase in our residential area. I live in Seven Oaks which is a large residential neighborhood located very close to this proposed installation.

Has a noise impact study been done to date? If so, what is the outcome of that study?

Sincerely,

Sonya Shea  
resident of Seven Oaks





3 December 2019

Tim Casey  
Cunningham Group Architecture, Inc.  
1030 G Street  
San Diego, CA 92101

Subject: Dignity Health, Mercy Hospital Southwest Expansion  
Exterior to Interior Noise Study

Dear Tim,

This report has been prepared to address the interior noise levels for the Dignity Health, Mercy Hospital Southwest Expansion project as it relates to exterior noise intrusion requirements set forth by the California Green Building Code (CALGreen). It is our understanding that while the Noise Control requirements of Section 1224 [OSHPD 1] apply to this project, Section 1207 (described in Section 1.2 below) of the 2016 California Building Code does not.

The project is located in Bakersfield, bounded by Stockdale Highway to the north, Old River Road and existing buildings to the west, CSUB Way to the east and a surface parking lot to the south. Exterior noise levels were measured between 13 November 2019 and 14 November 2019. Based on our measurements, described below, this project is not exposed to levels that exceed 65 dB(A)  $L_{eq}$  1-hr, and therefore is deemed to satisfy CALGreen 5.507.4 without requiring additional noise control/analysis. The fact that the exterior noise levels are below the CALGreen activation level must be clearly stated on the architectural plans.

## 1 EXTERIOR NOISE INTRUSION CRITERIA

As discussed above, the project is NOT subject to CBC 1207.1 and is below the 65 dB(A)  $L_{eq}$  1-hr trigger for CALGreen 5.507.4. The criteria for both are stated below for reference.

### 1.1 CALGreen Building Code

The CALGreen Building Code (Title 24, Part 11) outlines projects which must comply with Section 5.507.4 – Acoustical Control. These are defined as projects located within the 65 CNEL noise contour of airports or freeways or, in the absence of readily available noise contours, projects exposed to a noise level of 65 dB(A)  $L_{eq}$  1-hr during any hour of operation. Based on our measurements, this project is not exposed to levels that exceed 65 dB(A)  $L_{eq}$  1-hr, and therefore is deemed to satisfy CALGreen 5.507.4 without requiring additional noise control/analysis.

### 1.2 Patient Room Comfort Criterion – CNEL 45

It is our understanding that this 45 dB(A) criterion is NOT a Code requirement for this specific project, but is rather a 'comfort criterion' we would consider to be a reasonable balance between building cost and acoustical quality. We recommend that this 'comfort criterion' be applied for patient rooms in order to address patient comfort/building functionality. We have therefore assessed predicted noise levels in patient rooms against a 'comfort criterion' based on the California Building Code requirements for residential spaces, outlined below.

Section 1207.4 of the 2016 California Building Code stipulates that residential projects shall be designed such that, "...interior noise levels attributable to exterior sources shall not exceed 45 dB(A) [ $L_{dn}$  or CNEL] in any habitable room," with the additional note that interior noise levels should be analyzed based on the higher of the current noise levels or future levels predicted over a ten-year period. For the purposes of this study, we assumed a 1% average increase in traffic volume per year. This analysis is therefore based on the estimated future CNEL which would be 0.6 dB(A) higher than

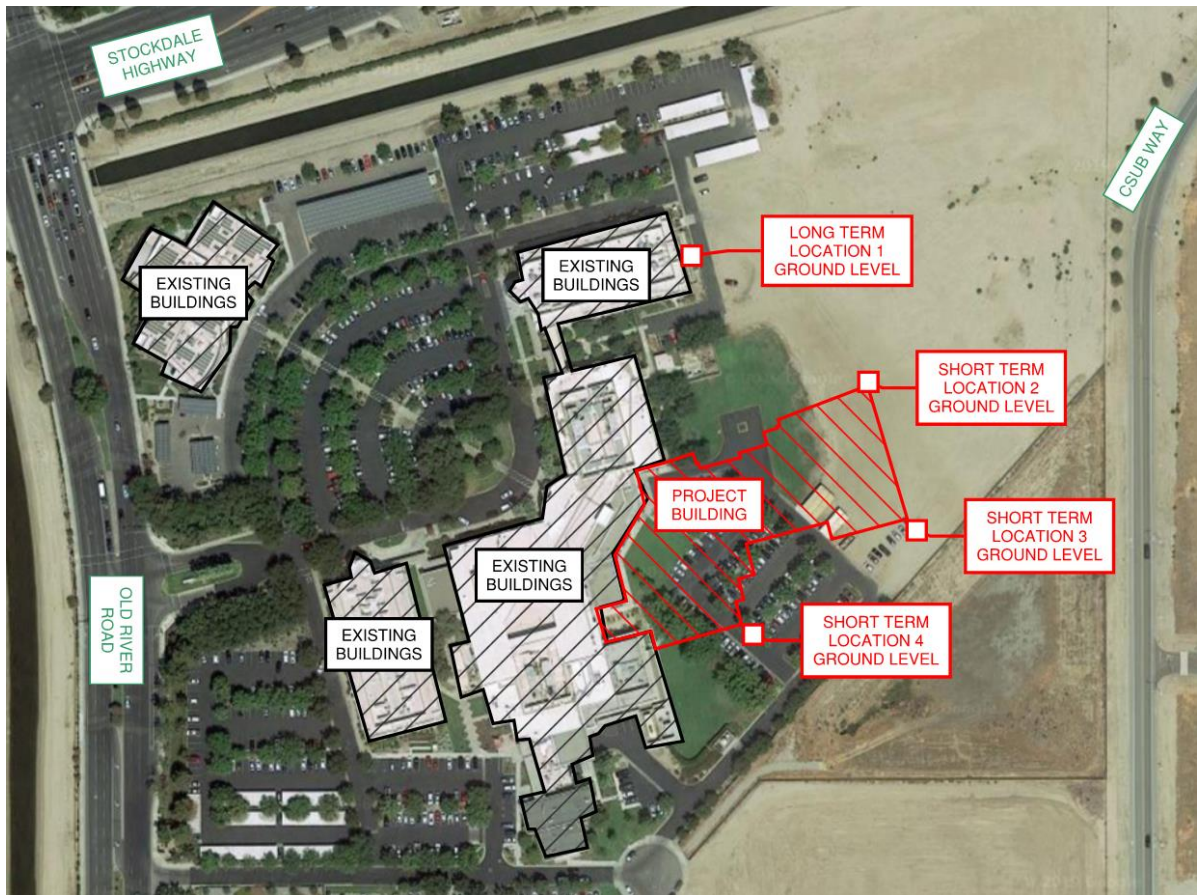
the measured value, assuming similar time-of-day traffic distribution and similar vehicle type mix to what was present during this investigation.

Please note that some experts, including the World Health Organization, have recommended noise intrusion limits as low as 35dB(A) for hospitals. Our experience, however, is that achieving such noise limits is not usually economically feasible for hospitals.

## 2 EXTERIOR NOISE MEASUREMENTS

### 2.1 Overview

Exterior noise levels were measured at the proposed site location between 13 November 2019 and 14 November 2019. During our survey, we performed continuous noise measurements at Location 1, shown in Figure 1, below.



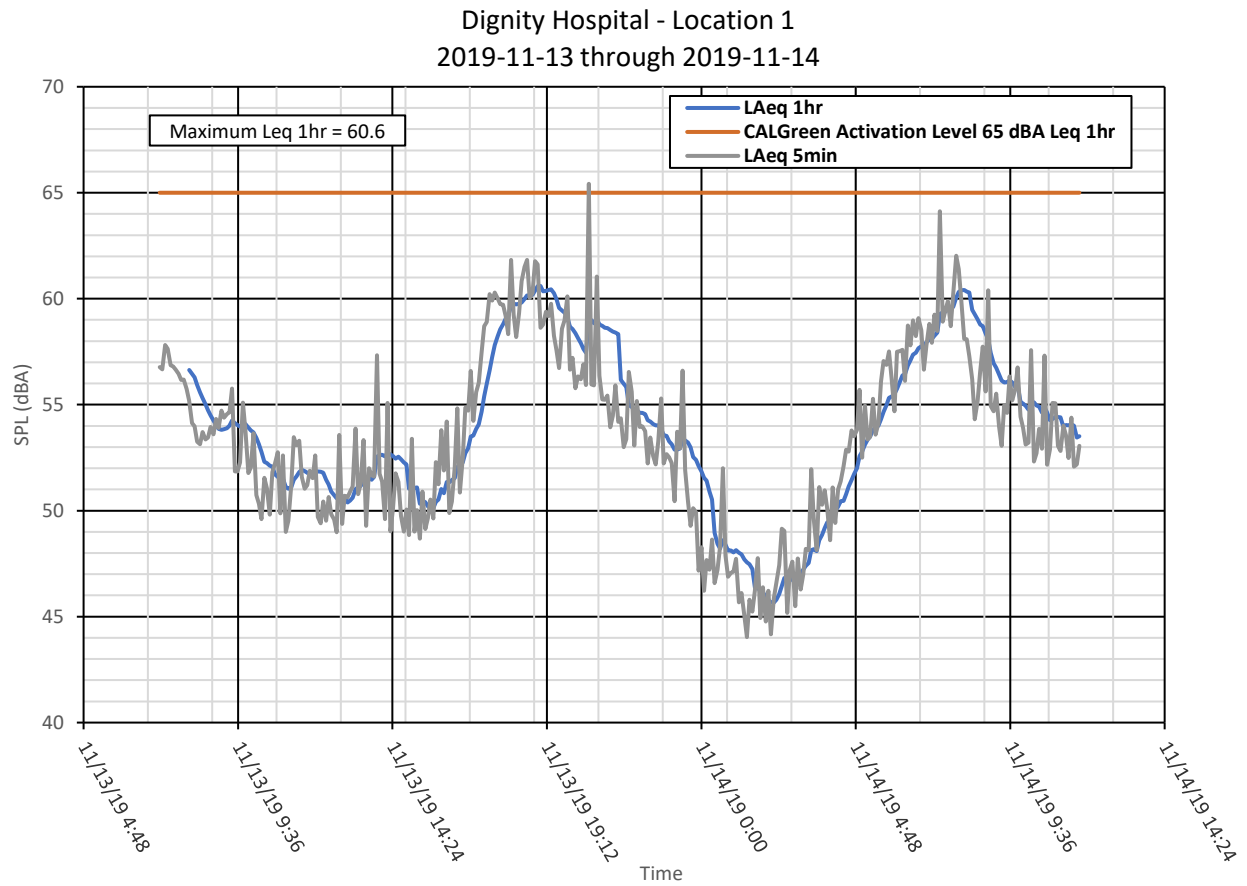
**Figure 1 – Measurement Locations**

The measurement microphone at Location 1 was placed on the exterior at the gas meter yard on the ground floor along the east façade of the existing project building facing Stockdale Highway and CSUB Way, shown in Figure 2, below. Continuous 5-minute measurement samples were taken between 7:10 AM on 13 November 2019 and 11:45 AM on 14 November 2019 and used to determine the loudest  $L_{eq}$  1-hr and CNEL for assessment against the CALGreen and CBC, respectively. Continuous measurement data is shown in Figure 3 in  $L_{eq}$  1-hr and Figure 4 in CNEL, below.



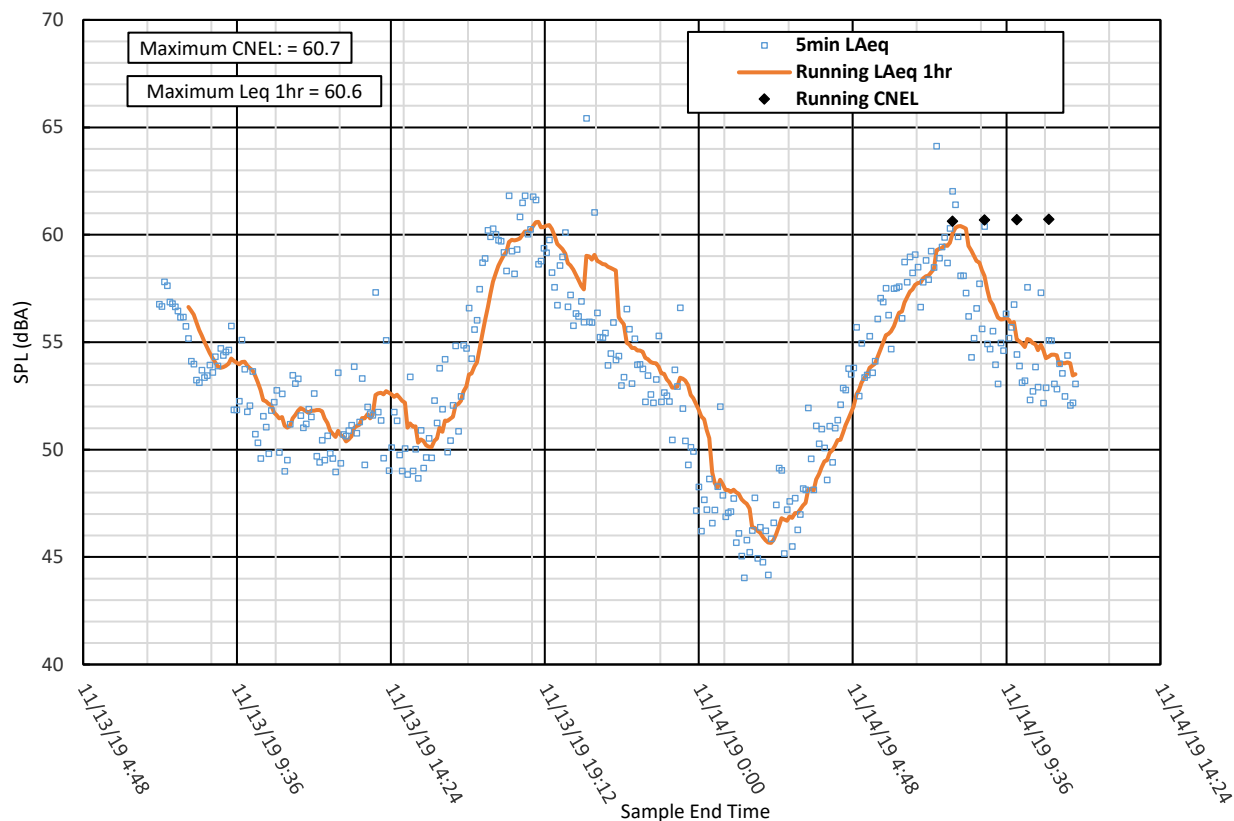


**Figure 2 – Long-term Measurement Location 1**



**Figure 3 – Long-term Location 1 Continuous Measurement Data Compared to CALGreen Activation Level**

Dignity Hospital - Location 1 - CNEL:  
2019-11-13 through 2019-11-14



**Figure 4 – Long-term Location 1 Continuous Measurement CNEL Data**

**2.2 CALGreen Reference Measurements (Loudest  $L_{eq}$  1-hr)**

The highest observed hourly average noise level at Location 1 was 60.6 dB(A)  $L_{eq}$  1-hr (measured between approximately 6:05 PM and 7:05 PM on 13 November 2019), including noise due to traffic and miscellaneous off-site environmental sources. The resulting spectrum is provided in Table 1, below.

**Table 1 – Location 1 Incident Exterior Noise Spectrum,  $L_{eq}$  1-hr**

Freq. [Hz]		63	125	250	500	1000	2000	4000	8000	dB(A)
Octave Band Levels	Location 1	63.8	63.2	58.5	55.4	57.6	53.1	38.6	24.1	60.6

**2.3 Comfort Criterion Measurements – Highest CNEL**

The loudest  $L_{eq}$  1-hr CNEL at Location 1 was determined to be 60.7 dB(A). The loudest  $L_{eq}$  1-hr spectrum at Location 1 (see Table 1) was adjusted as necessary to the loudest CNEL value. This spectrum has also been adjusted up by 0.6 dB(A) for our analysis to account for future traffic growth, as outlined in Section 1.2, and has been provided in Table 2, below.

**Table 2 – Exterior Noise Spectrum Adjusted to CNEL**

Freq. [Hz]		63	125	250	500	1000	2000	4000	8000	dB(A)
Octave Band Levels	Location 1	64.0	63.3	58.6	55.6	57.7	53.3	38.8	24.2	60.7
	Location 1 + 0.6 dB(A)	64.6	63.9	59.2	56.2	58.3	53.9	39.4	24.8	61.3

## 2.4 Short-term Measurements

Short-term (15 minute) measurements were taken at ground level along the proposed façade locations of the new proposed building (Locations 2, 3, and 4 shown in Figure 1, above) to supplement noise levels being measured simultaneously during the long-term measurement at Location 1. The measurement data from the short-term measurements was used to determine the incident noise spectra at the building facades as well as the difference in overall level between the difference in distance between the streets and the proposed building and the long-term measurement Location. The peak hour incident noise level spectra for Locations 2, 3 and 4 are given in Table 3 and the CNEL in Table 4, below.

**Table 3 – Spot Measurement Incident Linear Exterior Noise Spectra**

Freq. [Hz]		63	125	250	500	1000	2000	4000	8000	dB(A)
Octave Band Levels	Location 2	70.7	67.6	60.9	55.3	54.5	48.5	39.4	30.2	59.3
	Location 3	69.7	67.4	61.9	57.5	56.3	50.6	41.2	30.8	60.7
	Location 4	69.0	63.1	59.3	56.8	54.1	50.7	47.7	41.7	59.4

**Table 4 – Spot Measurement Incident Linear Exterior Noise Spectra Adjusted to CNEL**

Freq. [Hz]		63	125	250	500	1000	2000	4000	8000	dB(A)
Octave Band Levels	Location 2	70.8	67.8	61.1	55.4	54.6	48.6	39.6	30.3	59.4
	Location 2 + 0.6 dB(A)	71.4	68.4	61.7	56.0	55.2	49.2	40.2	30.9	60.0
	Location 3	69.8	67.5	62.0	57.6	56.5	50.7	41.3	30.9	60.8
	Location 3 + 0.6 dB(A)	70.4	68.1	62.6	58.2	57.1	51.3	41.9	31.5	61.4
	Location 4	69.1	63.2	59.4	56.9	54.2	50.8	47.8	41.9	59.5
	Location 3 + 0.6 dB(A)	69.7	63.8	60.0	57.5	54.8	51.4	48.4	42.5	60.1

## 3 NOISE REDUCTION ANALYSIS

### 3.1 CALGreen ( $L_{eq}$ 1-hr) Analysis

Per our measurements shown in Tables 1 and 3, the  $L_{eq}$  1-hr is below 65 dB(A) and therefore is deemed to satisfy CALGreen Code 5.507.4 without requiring additional noise control/analysis.

### 3.2 Comfort Criterion (Highest CNEL) Analysis

While we understand that the CBC Section 1207.4 is not a requirement for this project, we recommend using it as a guideline for in-patient room comfort. The highest 24-hour CNEL on the site was determined to be 61.3 dB(A) at Location 1, 60.0 dB(A) at Location 2, 61.4 dB(A) at Location 3, and 60.1 dB(A) at Location 4 all with the 0.6 dB(A) adjustment up for future growth (see Tables 2 and 4 above).

Typical Land Use Compatibility Guidelines, which are included in General Plan Noise Elements of various jurisdictions, state that 'normal construction' with closed windows will provide a minimum of 20 dB(A) of exterior to interior noise reduction. As such, interior noise levels, with closed windows and mechanical ventilation as required by Code, will conform to CNEL 45 in locations where the exterior noise exposure is CNEL 65 or lower. Therefore, per our measurements in Tables 2 and 4, which are below 62 dB(A), we anticipate that in-patient rooms will conform to CNEL 45.

### 3.3 Helicopter Analysis

While on site we observed a helicopter landing on site. We measured the noise levels of the helicopter taking off at ground level at a distance deemed safe by the helicopter pilot that lasted a period of approximately 2 minutes. We understand that helicopter landings will not be a regular/frequent occurrence. We recommend informing the client that helicopter landing/takeoff noise may result in a percentage of patients that experience sleep disturbance. We assume that to reduce the likelihood of patient sleep disturbance for such short periods (a few minutes per event) would be cost prohibitive. Please advise if the client would like additional analysis and recommendations to reduce helicopter noise to in-patient rooms.

## 4 CONCLUSION

Based on our measurements, described above, this project is not exposed to levels that exceed 65 dB(A) Leq 1hr, and therefore is deemed to satisfy the CALGreen Code Section 5.507.4. This must be clearly stated on the architectural plans.

While this project is NOT subject to CBC 1207.1, we still recommend using the Code as a guidance criterion for in-patient comfort. With standard construction and closed windows, we anticipate that the project will meet the recommended CNEL 45 comfort criteria.

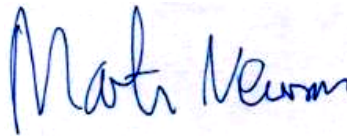
We also observed helicopters landing on site. We assume that construction to reduce noise from helicopters to reduce in-patient sleep disturbance is cost prohibitive.

We trust that this is adequate for your current needs. Please do not hesitate to contact us if you have any questions.

Yours Sincerely,  
**Newson Brown Acoustics, LLC**



Jonathan Kawasaki  
Consultant



Martin Newson  
Principal

CC. Sean Pak – Cuningham Group  
Noryn Lau – Cuningham Group



8 May 2020 - Updated 1 June 2020

Tim Casey  
Cunningham Group Architecture, Inc.  
1030 G Street  
San Diego, CA 92101

Subject: Dignity Health, Mercy Hospital Southwest Expansion  
Helipad Noise Study

Dear Tim,

This report has been prepared to address potential noise impacts on existing nearby noise-sensitive receptors due to the helipad proposed to be constructed as part of the Hospital's Conditional Use Permit Application.

The project is located 400 Old River Road in Bakersfield. An existing single-family residential development is located on the westerly side the Old River Road, and existing student dormitories are located easterly of the project on the CSU Bakersfield campus.

## 1 CITY NOISE CRITERIA

The Metropolitan Bakersfield General Plan, Chapter VII – Noise Element, contains guidelines “for protecting local citizens from the harmful effects of excessive exposure to noise.

As discussed on page VII-8 of the Noise Element, “Table VII-2 provides a method of determining land use compatibility for sensitive uses through the assignment of noise exceedance levels and time restrictions.” See Figure 1 below.

<b>TABLE VII-2</b>			
<b>NOISE LEVEL PERFORMANCE STANDARDS*</b>			
<b><u>Exterior Noise Level Standards</u></b>			
<b><u>Category</u></b>	<b><u>Cumulative Number of minutes in any one-hour time period</u></b>	<b><u>Daytime 7 a.m. to 10 p.m.</u></b>	<b><u>Nighttime 10 p.m. to 7 a.m.</u></b>
1	30	55	50
2	15	60	55
3	5	65	60
4	1	70	65
5	0	75	70
* Each of the noise level standards specified in this table shall be reduced by five (5) dB(A) for pure tone noises, noises consisting primarily of speech or music, or for recurring impulsive noises. These noise level standards should be applied at a residential or other noise-sensitive land use and not on the property of a noise-generating land use.			

Figure 1 – Noise Element Table VII-2

We have interpreted the “0 cumulative minutes in any one-hour time period” to be the maximum allowable ( $L_{max}$ ) noise level.

As discussed in the Implementation section of the Noise Element, on page VII-11, item 4 states: “Require proposed commercial and industrial uses or operations to be designed or arranged so that they will not subject residential or other noise sensitive land uses to exterior noise levels in excess of 65 dB CNEL and interior noise levels in excess of 45 dB CNEL and so that impacts on noise sensitive uses shall not exceed the performance standards in Table VII-2.” For reference, CNEL is a 24 hour, time of day weighted, average sound level which applies 5 dB and 10 dB weightings to the noise levels during evening and nighttime hours to account for higher sensitivity to noise during these hours.

## 2 HELIPAD NOISE ANALYSIS

It is our understanding that the proposed helipad will be utilized to transport hospital patients during emergency situations, which could occur at any time of night or day.

### 2.1 Noise Modeling

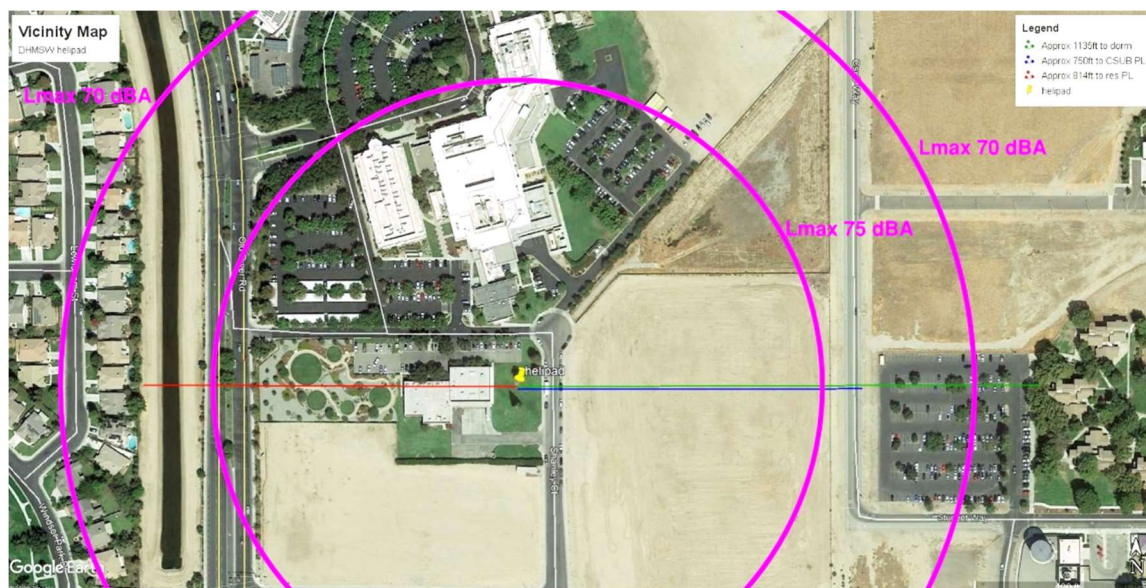
Noise levels due to the proposed helipad were computer modeled for the time that the helicopter is on the helipad immediately after landing and immediately before take-off, in terms of both the maximum ( $L_{max}$ ) noise level and the CNEL. The analyses assumed a total of four operations, i.e., two landings and two subsequent take-offs, daily, which we understand is expected to be the ‘usual’ regular number of daily operations. For our CNEL analysis, we have assumed that all four operations (two landings and two subsequent take-offs) would occur during nighttime (10 p.m. to 7 a.m.) hours. The results are discussed below, and are shown in Figures 2 and 3, for  $L_{max}$  and CNEL, respectively. Please note that the noise levels shown in Figures 2 and 4 do not include the beneficial acoustical screening of noise to the west which will be provided by the existing hospital Operations Building. Per the project drawings, the parapet of the Operations Building is 20’-0” higher than the surface of the helipad, and is located 38’-8” westerly of the helipad. See below for additional discussion of acoustical screening by the Operations Building.

The noise data used as the basis of the analysis was measurement data for a Bell 407 helicopter which were published in NPS Report No. GRCA-07-05, *Sound levels of helicopters used for administrative purposes at Grand Canyon National Park*, which was prepared for the National Park Service. It is our understanding that this Bell 407 helicopter is representative of the air ambulance helicopters that will utilize the proposed helipad.

### 2.2 Maximum ( $L_{max}$ ) Noise Levels

Figure 2, below, shows the locations of the  $L_{max}$  75 dBA and  $L_{max}$  70 dBA noise contours due to the helicopter when it is on the ground immediately after landing and before takeoff, without including acoustical screening by the existing Operations Building. It can be seen that the predicted maximum noise levels outside the existing residences located westerly of Old River Road, and outside the CSUB dormitories would both be lower than the  $L_{max}$  75 dBA daytime limit shown in Noise Element Table VII-2. Maximum noise levels outside the CSUB dormitories would also be lower than the  $L_{max}$  70 dBA nighttime limit shown in Noise Element Table VII-2. However, maximum noise levels outside some existing residences located westerly of Old River Road, without including acoustical screening by the existing Operations Building would briefly exceed the  $L_{max}$  70 dBA nighttime limit shown in Noise Element Table VII-2, by less than 5 dBA for helipad activity during nighttime hours.





**Figure 2 – Maximum Noise Levels due to Helicopter on Helipad  
(Not including acoustical screening by Operations Building)**

The existing Operations Building is located westerly of the helipad, and will acoustically screen noise transfer to the existing single-family residential properties located westerly of Old River Road, which would otherwise be exposed to maximum noise levels exceeding the nighttime  $L_{\max}$  70 dBA limit in Table VII-2. For reference, Figure 3, below, shows the easterly side of the Operations Building viewed from Shanley Ct.

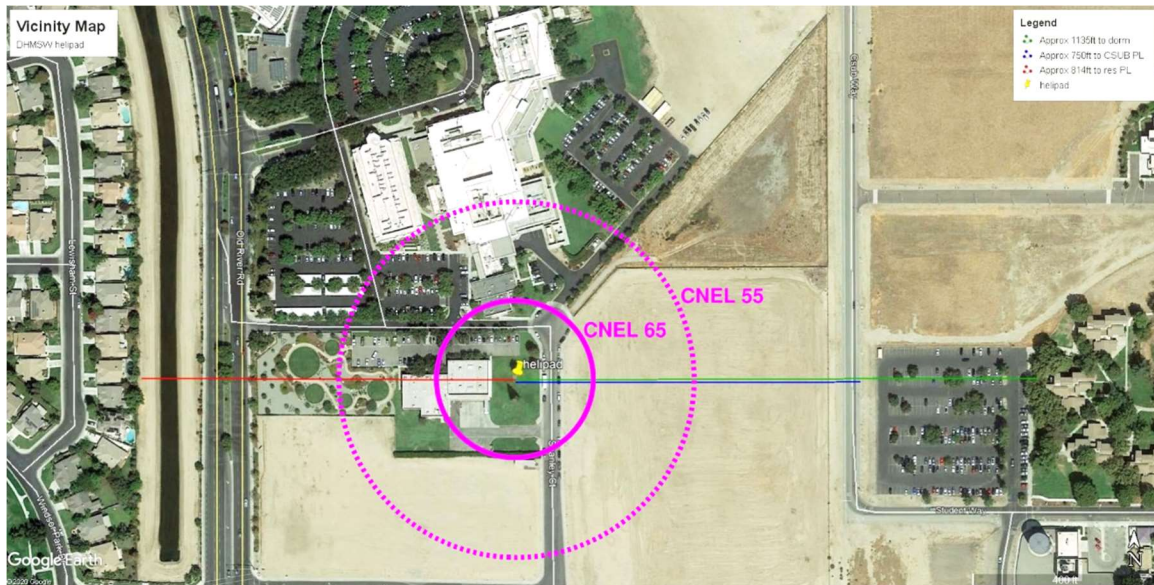


**Figure 3 – Easterly Side of Operations Building  
Viewed from Shanley Ct.**

**Figure 4 - Maximum Noise Levels due to Helicopter on Helipad Including Acoustical Screening by Intervening Buildings**, which is appended to this report, indicates areas where acoustical screening provided by intervening buildings will reduce exterior noise levels. In these 'acoustically screened' areas, the  $L_{\max}$  70 dBA and  $L_{\max}$  75 dBA curves are shown dashed for reference, the actual noise levels will be reduced to below the  $L_{\max}$  70 dBA nighttime noise limit shown in Table VII-2. It can be seen that all existing residential properties which are not acoustically screened by the Operations Building are located outside the  $L_{\max}$  70 dBA contour, and would also conform to both the daytime and nighttime maximum noise limits in Table VII-2. Therefore, noise impacts are less than significant.

### 2.3 CNEL

Figure 5, below, shows the locations of the CNEL 65 and CNEL 55 (24 hour time of day weighted average) noise contours due to the helicopter when it is on the ground immediately after landing and before takeoff. As discussed above, the calculation is based on two landings and two take-offs all occurring during nighttime hours. It can be seen that much of the area within the CNEL 65 contour is located on the hospital site. The CNEL 65 contour is located approximately 400 ft. from the easterly edge of Old River Road, and more than 500 ft. from the westerly edge of CSUB Way. The CNEL 55 contour is located approximately 200 ft. from the easterly edge of Old River Road, and more than 300 ft. from the westerly edge of CSUB Way.



**Figure 5 – CNEL due to Helicopter on Helipad  
(Not Including Screening by Operations Building)**

On the basis of the CNEL 65 contour location shown in Figure 5, noise at the existing residences located westerly of Old River Road, and at the CSUB dormitories located easterly of CSUB Way due to helicopters on the helipad would conform to the CNEL 65 noise limit discussed in the Implementation section of the Noise Element, on page VII-11, item 4.

Common Land Use Compatibility Guidelines state that typical residential construction with windows open for natural ventilation provides a minimum of 10 dBA of exterior-to-interior noise reduction. As such, noise-sensitive receptors located outside the CNEL 55 exterior noise contour would comply with the CNEL 45 interior noise limit discussed in the Implementation section of the Noise Element, on page VII-11, item 4, even with open windows.

Section 10. on page VII-13 of the Noise Element defines thresholds of significance for noise impacts (CNEL) due to mobile noise sources for a range of existing ambient noise (CNEL) levels. See Figure 6, below.

- An increase of the existing ambient noise level by 5 dB or more, where the existing ambient level is less than 60 dB CNEL;
- An increase of the existing ambient noise level by 3 dB or more, where the existing ambient level is 60 to 65 DB CNEL;
- An increase on the existing ambient noise level by 1.5 dB or more, where the existing ambient level is greater than 65 dB CNEL.

**Figure 6 – Noise Element Thresholds of Significance**



Based on our calculations, the noise exposure due to the helicopter on the helipad would be lower than CNEL 50 at both the existing single family residences located westerly of Old River Road, and at the CSUB dormitories located easterly of CSUB Way. The increase in CNEL due to the helicopter on the helipad would not exceed the threshold of significance for mobile noise sources.

As discussed above, long term time of day weighted noise at the nearby noise-sensitive receptors due to the proposed helipad operations, when the helicopter is on the ground, would conform to the CNEL 65 exterior and CNEL 45 interior noise limits (even with open windows) discussed in the Implementation section of the Noise Element, on page VII-11, item 4. Therefore, noise impacts are less than significant.

### 3 CONCLUSIONS

Maximum noise levels at all nearby noise-sensitive receptors, due to helicopters on the proposed helipad would conform to the  $L_{max}$  75 dBA daytime noise limits shown in Table VII-2. Maximum noise levels at noise-sensitive receptors located nearest the helipad, due to helicopters on the proposed helipad, are acoustically screened by the existing Operations Building and would also conform to the  $L_{max}$  70 dBA nighttime noise limit. Maximum noise levels at residences located in the northerly portion of the Tract westerly of Old River Road, which are not acoustically screened by the Operations Building, are located outside of the  $L_{max}$  70 dBA contour, and would also conform to the  $L_{max}$  70 dBA nighttime noise limit. Therefore, maximum noise impacts are less than significant.

The CNEL (24 hour time of day weighted noise level) due to helipad operations would conform to the CNEL 65 outdoor noise limit on page VII-11, item 4 of the Noise Element, and would also comply with the CNEL 45 interior noise limit at nearby noise-sensitive receptors even with open windows at the noise-sensitive receptors.

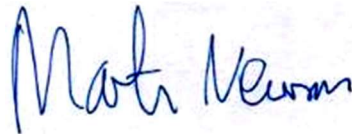
The increase in CNEL due to the helicopter on the helipad would not exceed the threshold of significance for mobile noise sources at either the existing single family residences located westerly of Old River Road or the CSUB dormitories located easterly of CSUB Way and is therefore less than significant.

We trust that this is adequate for your current needs. Please do not hesitate to contact us if you have any questions.

Yours Sincerely,  
**Newson Brown Acoustics, LLC**



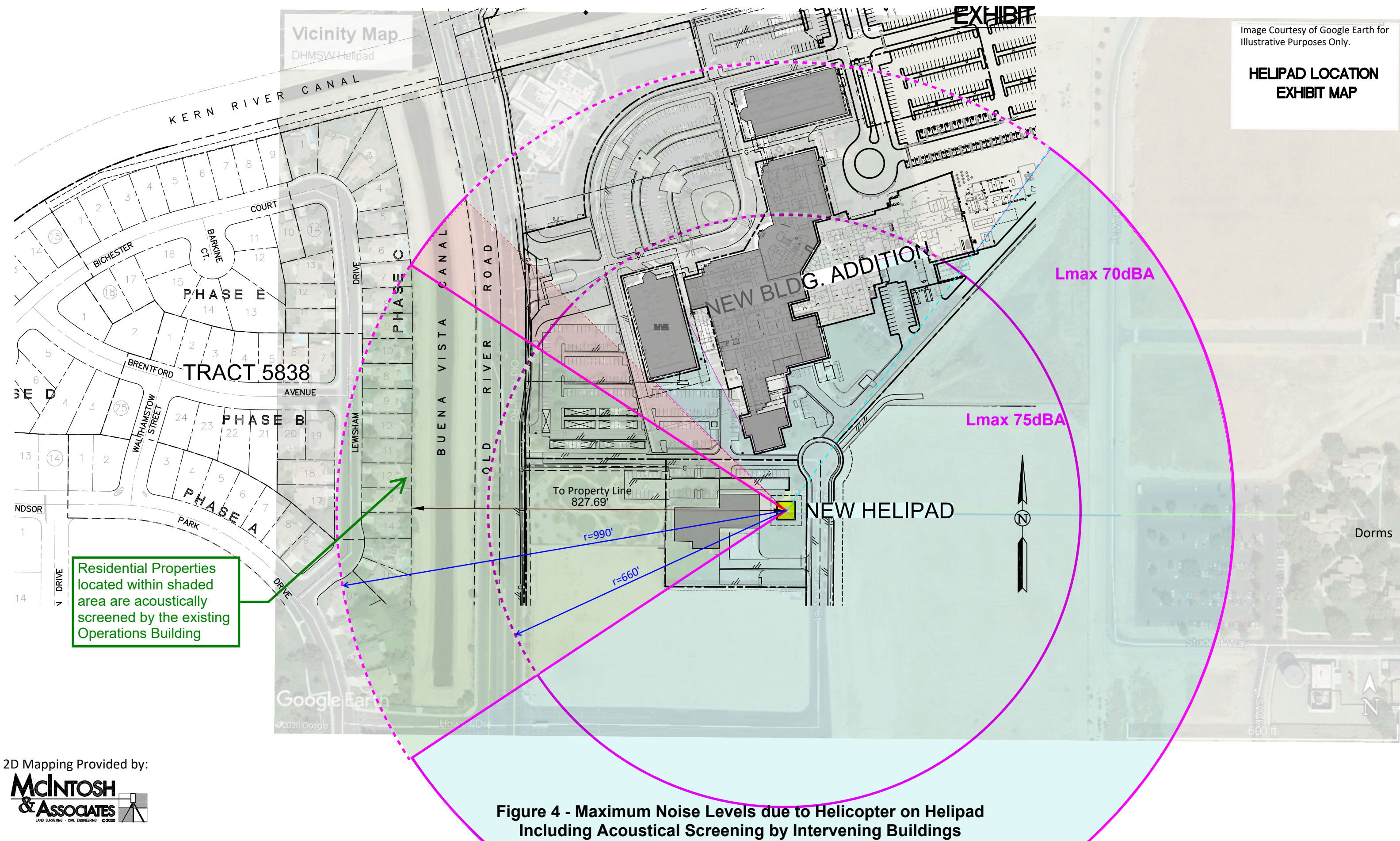
Joseph W. Celano, P.E.



Martin Newson  
Principal

CC. Sean Pak – Cuningham Group, Noryn Lau – Cuningham Group





2D Mapping Provided by:



**Figure 4 - Maximum Noise Levels due to Helicopter on Helipad Including Acoustical Screening by Intervening Buildings**