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# **Downtown Bakersfield Corridor Enhancement Master Plan**

Prepared By: IBI Group  
For: City of Bakersfield  
Status: FINAL

June 2021

## Project Purpose

The purpose of the Downtown Corridor Enhancement Master Plan (“The Plan”) is to establish design guidelines for streetscape improvements and gateways into and throughout downtown Bakersfield. The plan is inspired by the history and heritage of the community at large and the promise of the future as a high-speed rail station city. The Plan is a direct outcome and continuation of the Making Downtown Bakersfield Vision Plan and supports reinvestment in downtown infrastructure over the next 30 years to enhance the image and identity of Bakersfield, attract new businesses and residents, and improve walkability, livability and connection of key destinations.

### \*Goals

Project goals are driven by the Community Values and Goals established in the Making Downtown Bakersfield Vision Plan. These goals and values further inform more specific Project Objectives for streetscape improvements and define design parameters for the guidelines.

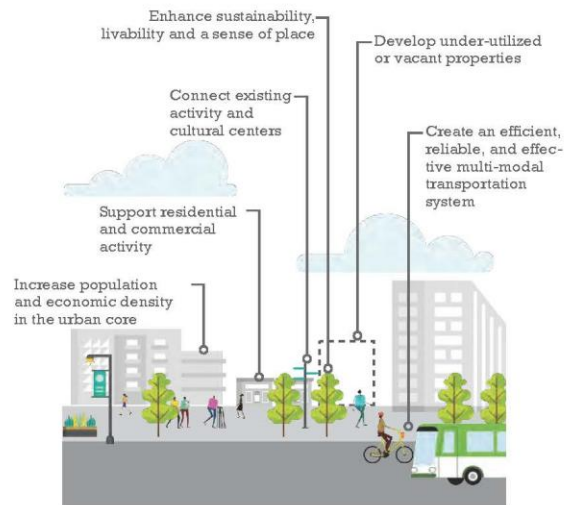


Exhibit 1: Project Goals

\*Project Goals & Community Values from the Making Downtown Bakersfield Vision Plan prepared by SOM for the City of Bakersfield

### \*Community Values

#### Livability

*Creating a comfortable, safe and engaging Downtown experience.*



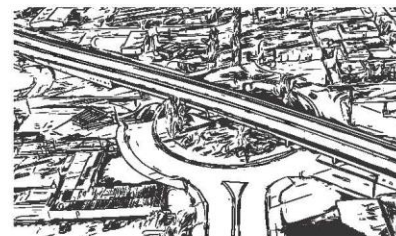
#### Connectivity

*Tying together the many Downtown assets.*



#### Prosperity

*Sharing the benefits of development across Downtown.*



### Objectives

1. Support mobility options.
2. Create a safe pedestrian environment.
3. Be adaptable and flexible.
4. Improve air quality, stormwater management and reduce the heat island effect.
5. Improve image and identity.
6. Minimize disruption to parking.
7. Walkable and easy to navigate.
8. Provide comfort and convenience.
9. Support existing business and attract new investment.
10. Create a unified and consistent streetscape.

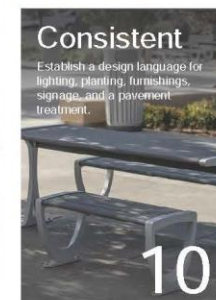
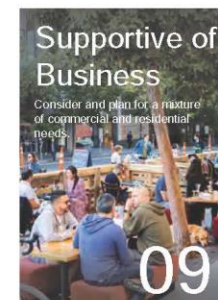
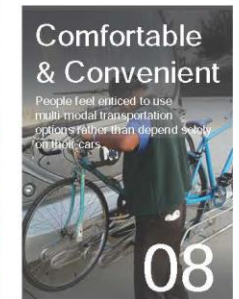
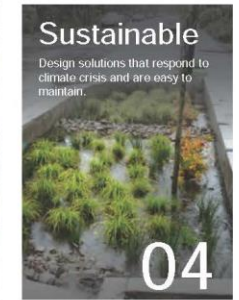


Exhibit 2: Project Objectives

## Districts and Nodes



As downtown continues to grow toward fulfilling the intent of the Making Downtown Bakersfield Vision Plan, the streetscape character will evolve with it. Driven by land use, transportation vision, existing destinations and high potential areas within the downtown, a framework of identifiable districts is emerging. These districts act as an organizing element which creates diversity of experiences and focus or concentration of streetscape improvements.

While districts organically form and are not absolute with boundaries, they tend to be catalyzed by signature features, destination facilities or special uses. Downtown Bakersfield, like many large cities, includes districts highlighted by civic uses, entertainment, shopping and dining, arts and culture, health and wellness and heritage and history. In most districts, one can typically find a special intersection or "node" with a high concentration of the district character or uses.

These nodes can become iconic locations to celebrate a district identity. The streetscape becomes the constant element that connects all districts and displays the character of each node.



Bakersfield Convention and Visitors Bureau (Visitors Center) Photo: Courtesy of Bakersfield CVB

### DOWNTOWN DISTRICTS

#### Identity

These Districts, identified by the Downtown Business Association plus 11 local Downtown hospitals, illustrate the recent identity of a revitalized Downtown. Their concentration around Chester Avenue and 21st Street denotes a "core" and demonstrates the many definitions of what constitutes "Downtown". Growing, engineering and creating new Districts will further embrace the diversity of Downtown's identity with its many services, amenities and destinations.



Building on Districts Identified in the Making Downtown Bakersfield Vision Plan



Wall Street - Arts, Culture & Entertainment District



Mill Creek Linear Park & Entertainment District



Chester Avenue & 21st Street Station District



H Street - Padre Hotel & Fox Theater Arts, Culture & Entertainment District

## Streetscape Inspiration

The downtown Bakersfield streetscape shall be safe, consistent and functional, yet deserves to also be iconic and unique to the community. To be iconic, the streetscape must be experiential, inspiring and somewhat unexpected. That is to say - it is much more than simply a sidewalk. On the contrary, the pedestrian streetscape experience should be educational and special to the place.

The Plan is inspired by the local heritage and history of agriculture, railroads and the music culture that is unique to Bakersfield. It is also intended to be fresh and clean to represent the promise of the future, as a destination and high-speed rail station city, attracting new businesses and residents.



Agriculture - Rhythm of Almond Groves (Photo: www.flickr.com)



Rail History & Future - Rhythm of the Rail-yards (Photo: autorio www.pzhang.com)



Music History & Future - Rhythm of the local Bakersfield Sound (Photo: www.pizabay.com)



Agriculture - Rhythm of the Crop Fields (Photo: Jim Ekstrand, www.stock.adobe.com)

### Inspiration

The design inspiration translates into unique patterns derived from the beat of the Buck Owens' song *Streets of Bakersfield*, the rhythm of rail networks, and the consistency and repetition of crop fields. The pattern becomes the "constant hum" of the streetscape customized to the community where the nodes become the notes of the musical score representing the Bakersfield Sound and The Sound of Something Better.

The *Streets of Bakersfield* song was written by Homer Joy



## Anatomy of a Streetscape

Streetscape is the backbone of the public realm and connective tissue that ties together the open space network of downtown Bakersfield. Specifically, the pedestrian street environment is made up of several key components that organize its function and use.

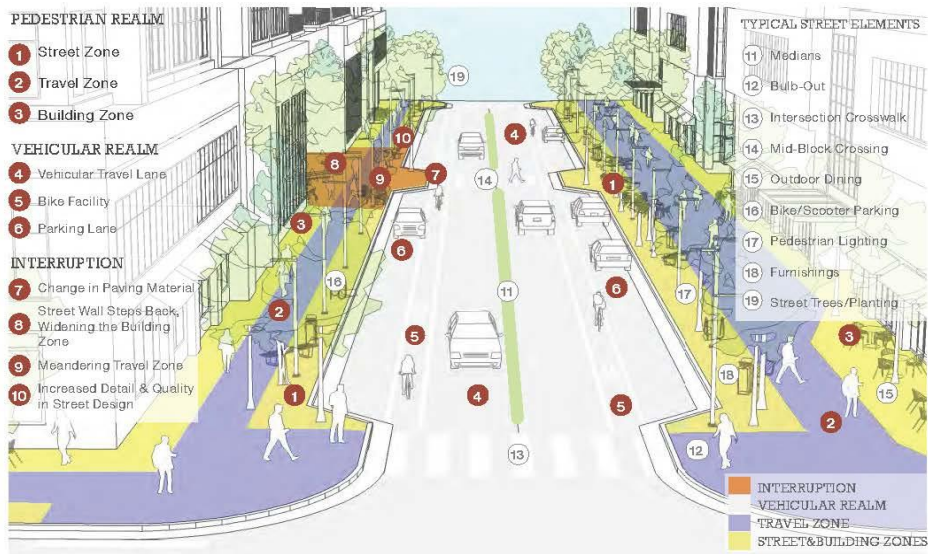


Exhibit 21 - Anatomy of a Streetscape

### 1 The Street Zone

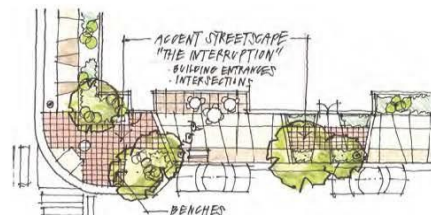
The area immediately behind the street curb, identified here as the Street Zone, separates and buffers pedestrian circulation from vehicular traffic and parking. This zone is particularly important and multi-functional where many components are located such as lighting, furnishings, street plantings and specialty features.

### 2 The Travel Zone

The Travel Zone is the unobstructed travel path for pedestrians and is flexible and accommodating for free pedestrian movements.

### 3 The Building Zone

The building zone acts as the transition zone from travel way into adjacent businesses or properties. When space allows, this zone may be utilized for outdoor seating or dining, displays or seasonal opportunities for businesses to improve visibility.



## Streetscape Design Principles

Streetscape design should create a rhythm through predictable patterns, flow and materials that are experienced while traveling through Bakersfield. This consistency in streetscape design is a key approach in achieving a strong identity. This constant hum of streetscape elements lends identity and predictability to the pedestrian experience and reinforces the image of Bakersfield.

The streetscape design toolkit will address the elements and patterns needed to establish a consistent image. Standards will define a pattern and palette of materials from paving, lighting, planting strategy and street furnishings to amenities, spacing, and general pedestrian realm layout. The toolkit allows for the implementation of consistent streetscape elements throughout the downtown area on a project by project basis, providing enough flexibility to respond to various site conditions and proposed development.

### Branding & Wayfinding

Wayfinding helps pedestrians, cyclists and motorists navigate their communities efficiently and safely. Effective wayfinding strategies will help the community know where they are, where they are going and how to get there. Wayfinding may be achieved through material choices, signage or digital aids. The focus of the toolkit with regards to wayfinding, will be the selection of materials to assist with wayfinding as well as banner programs to assist with community identity. A comprehensive wayfinding study is recommended for strategic placement.



Downtown San Jose, CA - Banner Signage Program



Directional Wayfinding Signage assists pedestrians with navigation.



Directional curb ramps and hardscape material selection provides legibility, accessibility and assists with wayfinding in the public realm.

### Accessibility

Barrier-free access within the public realm is critical to a successful design and will provide continuous, safe and unobstructed paths of travel to local destinations and transit stops to facilitate greater independence of not only persons with disabilities and the elderly but also reduce barriers for people facing other challenges navigating the public realm such as delivery persons or families with young children.

Consistent treatment of paving materials, curb ramps, and clear pedestrian zones shall be consistent with ADA standards and the overall vision for the streetscape improvements.

## Streetscape Components

Ultimately, the streetscape experience is derived through various combinations of individual components depending on the street typology or specific right-of-way conditions or constraints. Regardless of condition, the streetscape components are generally defined within three categories; hardscape, vertical features and furnishings.

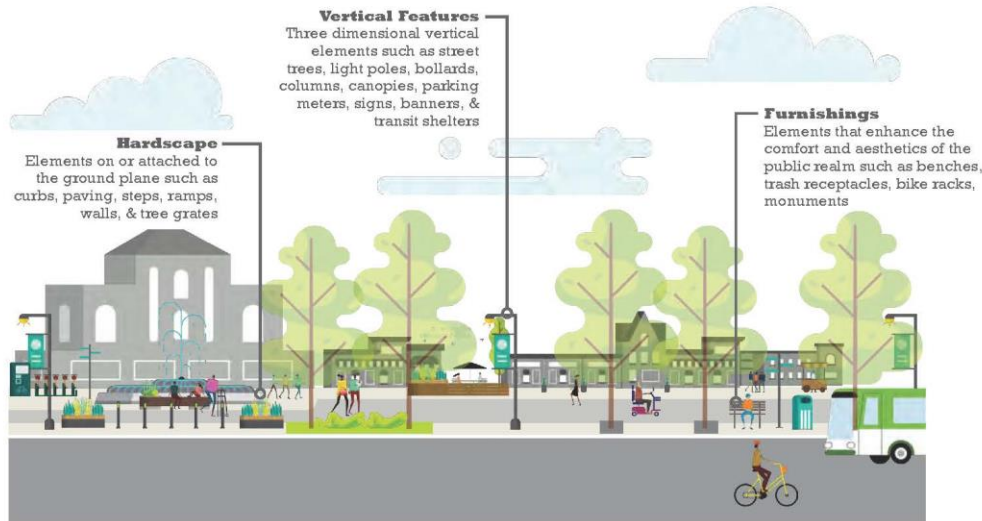
### Hardscape

Hardscape on the ground plane provides a physical connection to place. When designed thoughtfully it also provides organization and informs pedestrian flow. Hardscape has the ability to be both interpretive and informational.

Textures, both tactile and visual, as well as materiality are important to provide character and narrative. Local history becomes evident through the use of local materials. It is equally important to consider micro-climate in the selection of materials. The color, whether dark or light impacts the temperature and has the ability to mitigate heat island effect or conversely provide a sense of warmth in a cooler environment.



Vertical Feature - Public Art Installation  
Growth Rings by Oscar Tuazon - Central Wharf Park, Boston, MA  
(Photo by Bob O'Connor)



### Vertical Features

Vertical features provide three-dimensional definition to the public realm. They include such items as light poles, bollards, columns, canopies, parking meters, signs, banners & transit shelters.

Their purpose could be to provide information, shade, and/or aesthetics. They contribute to the mood of the space or a person's sense of safety. Vertical features provide color to the public realm and assist with branding and wayfinding efforts, as well as contributing to the character of the public realm.

### Furnishings

Furnishings enhance the public realm by providing sources of comfort & convenience. Elements such as benches, trash receptacles and bike racks provide comfortable places to sit and linger, allow for the ability to keep a space clean, and provide convenient, secure parking for cyclists. They provide order and clarity and contribute to people's comfort.

Continuity and repetition in color and material of site furnishings contributes to the character and sense of place. Colors and materials draw inspiration from the local vernacular and are used to express and support the unique characteristics of neighborhoods or districts.



Vertical Features & Furnishings - Cumberland Avenue, Knoxville, TN  
(BI Project - Photo by Denise Retallick)



Furnishings - Big Belly Trash Receptacle  
(www.bigbelly.com)



Hardscape - Tree Grates  
(www.irongrates.com)



Vertical Features - Public Art Installation  
Carrosse by Xavier Veilhan, Versailles, France  
(Photo: www.pixabay.com)



Vertical Features - Signage  
Worcester, MA  
(Photo: www.sellertperkins.com)



Furnishings  
Freiburg, Germany  
(Photo: www.pixabay.com)



Vertical Features & Furnishings - Public Art Installation - Pedestrian Paseo  
Giuseppe Mazzini Street, Ferrara, Italy  
(Photo: www.pinterest.co.uk)

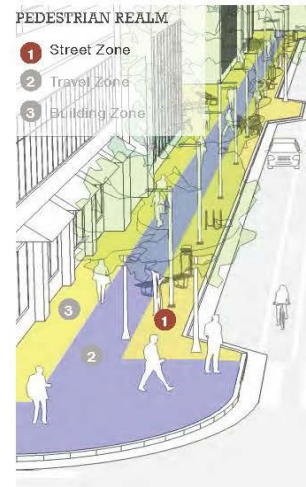
## Street Zone Materials and Furnishings

The family of materials and furnishings for the street zone within the Downtown area and the Chester Avenue and H Street Corridors is unique to Bakersfield and is inspired by the Bakersfield Sound.

The street zone will include, unit pavers, concrete paving & curbs, trash and recycling receptacles, benches and alternate seating, bollards, planting, tree grates, signs & banners, and street light standards. Street and pedestrian lights should consider Dark Sky compliance and solar technology.

Public art is a strong focus in Bakersfield and will be at home in the street zone as well as the building zone. Public art may also be incorporated in bulb-outs where space and sight lines allow. Ground plane public art is appropriate at bulb-outs and intersections but vertical elements will require review on a case by case basis.

To compliment the local character and achieve a design that is unique to Bakersfield, the paver layout within the street zone has been designed using parametric technology to mimic the sound wave of the song "The Streets of Bakersfield" by Buck Owens and Homer Joy. This unique feature celebrates Bakersfield and provides a distinctive material palette linking the various districts in support of a strong community identity.



## Seating



Traditional Bench Style (Precidio - 4 seat by Landscape Forms)  
Default Color - Basic Black  
Multicolor Options by District



Precedent: Traditional Bench Cumberland Avenue Knoxville, TN.



Optional Combination Bench/Tree Guard at Street Tree ('Alcorque' by Lab23 or similar)  
Default Color - Corten Steel



Cafe Tables may be placed in the street zone where the street zone is 5' wide or greater.



Patios may be located in the street zone where the street zone is 8' wide or greater.

## Trash & Recycling Receptacles



Traditional Trash/Recycling (Plexus II by Landscape Forms)  
Default Color - Black  
Multicolor Options by District



Precedent: Custom Big Belly Trash Receptacle San Francisco



Solar Trash/Recycling Compactor w custom panel (Big Belly or similar)  
Default Color - Black

## Shade Structures



Custom Shade structure designed by Copley Wolf Design. The design evokes the sense of a railroad and is in keeping with the rhythm of the design for Bakersfield.

Where shade is desired and trees are not feasible custom shade structures may be appropriate as a specialty feature. Proposed structures should compliment the Sounds of Bakersfield theme and local history and be focused on areas of interruption and public gathering. Implementation would be at the discretion of the City of Bakersfield.

Posts and other obstructions must meet with ADA standards, be located outside of the travel zone and consider site lines for business signage and vehicular traffic.

## Bike & Scooter Racks



Iconic Bike Rack ('Bike' by Dero or similar)  
Default Color - Basic Black  
Multicolor Options by District



Thematic Bike Rack ('Icon Hitch' by Dero or similar)  
Default Color - Basic Black  
Multicolor & Theme Options by District



Scooter Rack ('E-Scooter' Rack by Dero or similar)  
Default Color - Basic Black  
Multicolor & Theme Options by District

\*Note: Both the Bike and Icon Hitch Bike Racks are available with custom Bakersfield logo.

## Public Art



Public art is often located in the building zone or other specialty areas, however the street zone provides an opportunity for narrow objects and sculptures to animate the streetscape.

## Signs, Banners & Flags

**Comprehensive Signage & Wayfinding**  
This toolkit should be used to prepare a comprehensive signage and wayfinding plan for implementation.

**Bldg. Zone**    **Travel Zone**    **Street Zone**



Custom Colorful Banners can help districts celebrate uniqueness and local historical properties. Banners may be one per pole or two per pole where appropriate. Bakersfield typically installs two banners per pole.



Street information signs provide wayfinding, assist with branding and allow for event notification at a pedestrian scale.

Pole or column formats fit well in the street zone and provide an opportunity to celebrate districts.



Concrete markers provide both seating and wayfinding in the public realm. (K.M.0 by Escofet or similar)



Directional/distance signs allow pedestrians to make informed choices and encourage walkability.



## Paving Materials & Conceptual Design

**Street Zone Paving Pattern (width varies)**  
Where space allows, the paving pattern for the street zone shall be a sound-wave derived from the song 'The Streets of Bakersfield' through the use of parametric design. The design is unique to Bakersfield and is achieved through the application of two contrasting colors of 6x12 pavers. Pavers must be ADA compliant and installed with consideration for local soils and existing basements within the right-of-way within the historic core area.

**18" Concrete Maintenance Band**  
**6x12 Pavers**  
**Travel Zone (7' minimum)**  
**Building Zone (width varies)**  
**12x12 Pavers**  
**Concrete ADA Curb Ramps**

Unique pattern produced through parametric design. Site specific parameters to be used to produce detailed layout of contrasting color pavers.

Graphic Illustration (not to scale) of typical pattern using 6 x 12 pavers.

Graphic Illustration (not to scale) of typical pattern using 12 x 12 pavers at intersections.

**Custom Medallion**

\*Note: Pavers shall be sand set over 4" thick concrete sub-base. Where basement locations or other underground obstructions occur, alternative methods of paving installation will be required on a case by case basis.

## Planting Design

### Planting Objectives

The objectives of the planting design are to create inviting spaces that are both aesthetically pleasing and supportive of a healthy urban environment.

The Downtown Bakersfield plan organizes the planting strategy objectives as follows:



**Provide Visual Interest:** Frame the pedestrian and vehicular zones with planted ornamentation.



**Support Human Experience:** Pedestrian comfort is a high priority, provide shade, interest and separation from business activities and the street.



**Create a Healthy Climate:** Reduce heat island, mitigate air pollution and provide noise reduction.



**Promote a Healthy Urban Forest:** Support community vibrancy through trees and plants that have room to grow and succeed.

## Planting Design Strategy

The planting objectives will be met through the following strategies:



**1. Clearly define use areas spatially, providing a softening contrast to hardscape and delineating downtown with an organized planted streetscape scheme.**



**2. Street Zone: plantings along with furnishings, create a buffer between vehicles and pedestrians. Transitioning from the street to the pedestrian sidewalk (travel zone).**



**3. Reduce the urban heat island effect and mitigate energy costs related to air temperature by planting trees and vegetation.**



**4. Select species that are proven top performers in the area and urban environment.**



**6. Promote healthy tree growth by providing substantial topsoil and sufficient soil volume.**

## Planting Palette

The right plant in the right place! The recommendations are intended to be guides. Consideration to specific soil conditions, adjacent materials and potential obstructions shall be considered when placing individual plants

### Large Shade Tree Species Planting Location Matrix

Visualization	Species	Tree Grate / Planters	Medians	Street Zone	Street Orientation & Preferred Locations
	Deodar Cedar <i>Cedrus deodara</i>	No	No	No	Gateways and/or large open spaces.
	Ginkgo <i>Ginkgo biloba</i>	Yes	Yes	Yes	North/South Streets, Complete Streets, Arterials and Collectors
	London Plane <i>Platanus acerifolia</i>	No	No	Yes	North/South Streets, Complete Streets, Arterials and Collectors
	Escarpment Live Oak <i>Quercus fusiformis</i>	Yes	No	Yes	All orientations. Arterials and Collectors
	Black Locust <i>Robinia pseudoacacia</i>	Yes	No	Yes	East/West Orientations. All street types.
	Chinese Elm <i>Ulmus parvifolia</i>	Yes	Yes	Yes	All orientations and street types, good for commercial frontages.
	Sawleaf Zelkova <i>Zelkova serrata</i>	Yes	Yes	Yes	East/West Orientations. All street types.

Table 1: Large Shade Tree Species Planting Location Matrix

## Chester Avenue (Typical Tree Spacing Examples)

California Avenue to 4th Street



Exhibit 13 - Chester Avenue California Ave to 4th Street - Typical Plantings

4th Street to SR-58

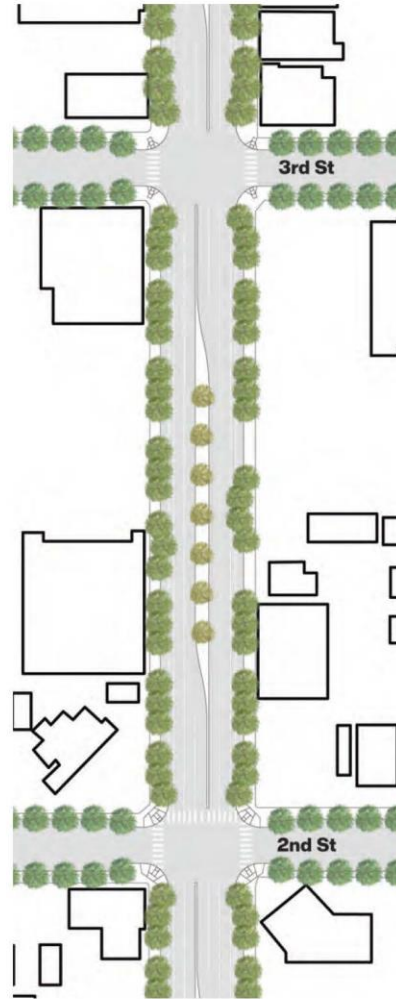
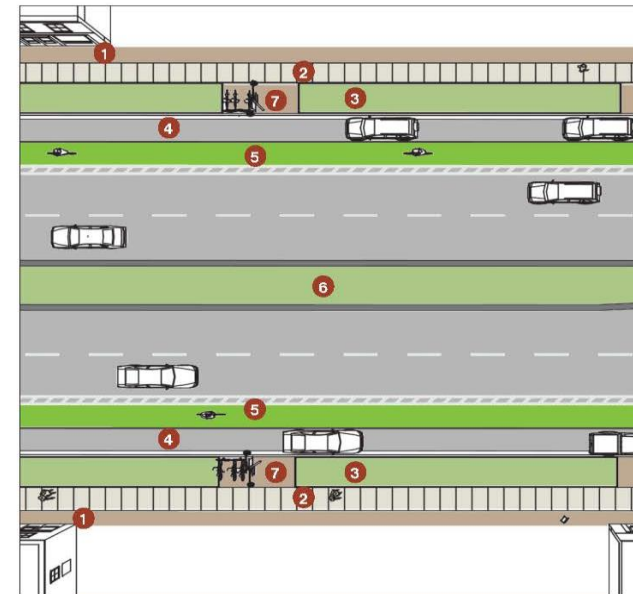


Exhibit 14 - Chester Avenue 4th Street to SR-58 - Typical Plantings



Exhibit 21 - Section - Typical Non Urban Condition with 85' Curb to Curb



TYPICAL STREET ELEMENTS

- 1 Building Zone
- 2 Travel Zone
- 3 Street Zone
- 4 Parking Lane
- 5 Bike Lane
- 6 Median
- 7 Streetlight with Banner

Exhibit 22 - Plan - Typical Non Urban Condition with 85' Curb to Curb



## Chester Avenue

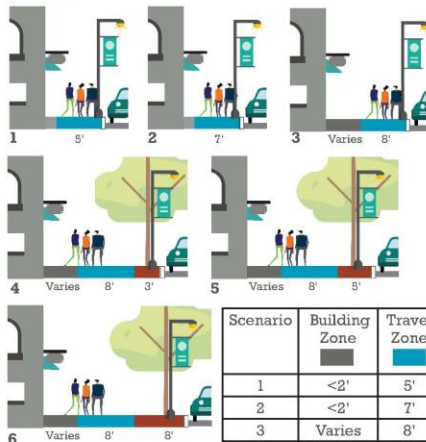
The design for Chester Avenue focuses on bulb outs and sidewalk improvements as a way to improve the pedestrian experience, calm traffic and create a sense of place. Tree and furniture spacing increase in density towards the Downtown creating a rhythm designed to calm traffic in stages, first at 4th Street and again at California Avenue as travelers move northward towards the core. Similarly the pattern increases in density traveling south on Chester Avenue from Garces Circle moving towards the historic core.

### CHESTER AVENUE RECOMMENDATIONS

Existing Streets	N/A
Typical Right-of-Way Width	112.75' to 116.5'
Typical Curb to Curb Width	85' (1st to 4th), 81' (4th to 11th), 86' (11th to 29th)
<b>Vehicular Realm</b>	
Travel Lanes	4 lane maximum, 2 in each direction + center turn lane
Lane Width	11' to 12'
Allowable Turn Lanes	Left turn center median
Parking Lanes	Parallel parking both sides. Accessible spaces to be provided in accordance with ADA.
Median	11' to 14'
Bicycle Facilities	5' to 5.5' Class II
Specialty Paving †	Required at crosswalks, recommended for bike lanes
<b>Pedestrian Realm</b>	
Streetscape Width ‡	7' to 17'
Minimum Sidewalk Width	8' minimum travel zone shall be implemented where feasible. 5' or 7' travel zones may be used only where existing conditions prohibit the implementation of wider sidewalks.
Street Zone Width	Min. 3', 5', or 8' both sides. The street zone must be 3' or more OR it should be eliminated in favor of a curb face travel zone.
Tree Type ‡	Deciduous
Tree Layout ‡	Exhibits 9 to 14
Specialty Paving †	Concrete pavers or decorative concrete in street zone and building zone. Concrete sidewalk in travel zone.
Required Furnishings	Bollards at intersections, bike racks, benches, trash receptacles, pedestrian and vehicular light poles.
Recommended Furnishings	Banner poles, tree grates or tree guards, public art.

**Notes**

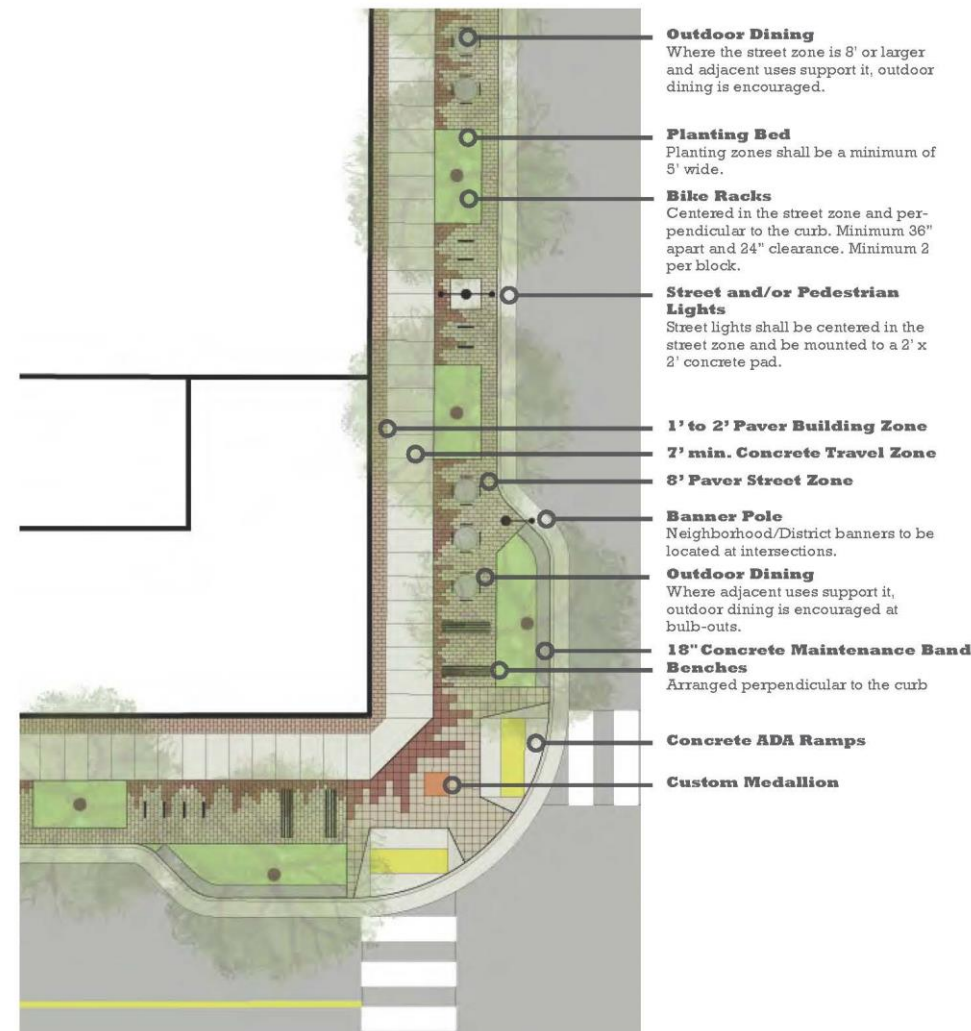
- 1-Refer to the Street Zone Materials & Furnishings Section
- 2-Refer to the Planting Design Section
- 3-Existing conditions will require alternatives to the preferred section in certain locations. Curb face travel zone shall be implemented where necessary to accommodate existing large trees, buildings or other obstructions that are not easily relocated.
- 4-Crosswalks occur only at stop or traffic light-controlled intersections. Crosswalks shall be continental in style unless otherwise requested by the City of Bakersfield. Mid-block crossings for Wall Street shall be an exception.
- 5-All crossings must include ADA ramps per City of Bakersfield standards. Where feasible, directional ramps offer greater accessibility and wayfinding for pedestrians.
- 6- Mid-block-bulb outs are encouraged to provide space to tree planting where space for a minimum 3' street zone is not available. Bulb-outs also encourage traffic calming.
- 7-Street zones may be paved or landscaped depending on context and adjacent uses. Trees in tree grates should be implemented in areas of high pedestrian activity. In areas of low pedestrian activity, street zones with permeable materials are encouraged, either vegetated or permeable surfacing.



Scenario	Building Zone	Travel Zone	Street Zone
1	<2'	5'	-
2	<2'	7'	-
3	Varies	8'	-
4	Varies	8'	3'
5	Varies	8'	5'
6	Varies	8'	8'

## Typical Intersection Urban Condition

### Bulb-Out with 8' Street Zone



**Outdoor Dining**  
Where the street zone is 8' or larger and adjacent uses support it, outdoor dining is encouraged.

**Planting Bed**  
Planting zones shall be a minimum of 5' wide.

**Bike Racks**  
Centered in the street zone and perpendicular to the curb. Minimum 36" apart and 24" clearance. Minimum 2 per block.

**Street and/or Pedestrian Lights**  
Street lights shall be centered in the street zone and be mounted to a 2' x 2' concrete pad.

**1' to 2' Paver Building Zone**  
**7' min. Concrete Travel Zone**  
**8' Paver Street Zone**

**Banner Pole**  
Neighborhood/District banners to be located at intersections.

**Outdoor Dining**  
Where adjacent uses support it, outdoor dining is encouraged at bulb-outs.

**18" Concrete Maintenance Band Benches**  
Arranged perpendicular to the curb

**Concrete ADA Ramps**

**Custom Medallion**

### Public Survey

#### Questions:

- Do you prefer Bakersfield Streetscape to have a historic or modern aesthetic?
  - Historic
  - Historic, Mixture of Both
  - Modern
  - Modern, Mixture of Both
  - Mixture of Both
  - Blank
- Do you prefer Bakersfield Streetscape focuses on the rail history or music history?
  - Music History
  - Rail History
  - Both
  - Agricultural History
  - Oil and Agriculture
  - Other
- Do you prefer decorative paving or standard concrete walks to be used along major streets (H Street, Chester Avenue and Q Street to name a few) to support an identifiable and consistent street design?
  - Concrete
  - Pavers
  - Mixture of Both
  - No Preference
  - Blank
- Do you prefer palm trees or shade trees?
  - Palm Trees
  - Shade Trees
  - Mixture of Both
  - Other
  - Blank
- Do you prefer shade cast by shade structures or shade trees?
  - Shade Trees
  - Shade Structure
  - Mixture of Both
  - Other
  - Blank
- Do you prefer traditional seating or more artful/whimsical seating options to support the objective of improving the comfort and convenience of Downtown Bakersfield?
  - Whimsical Seating
  - Traditional Seating
  - Mixture of Both
  - Other
  - Blank
- Do you prefer event nodes or open space along Downtown Bakersfield's streets?
  - Programmed Nodes
  - Unprogrammed Nodes
  - Mixture of Both
  - Don't know
  - Other
  - Blank
- Do you prefer natural planting schemes or more formal planting schemes to support a sustainable Downtown Bakersfield street design?
  - Natural Planting
  - Formal Planting
  - Mixture of Both
  - Other
  - Blank

### Project Workshop 2

December 10, 2020 @5:00pm PST.

#### Attendees:

City of Bakersfield: Navdip Grewal, Brianna Carrier, Dianne Hoover, Cecelia Griego, Stuart Patterson  
 Local Consultants: Troy Hightower, Mandy Freeland  
 Researcher: Adam Cohen  
 Kern County: Greg Collins  
 Local Real Estate Broker: AJ Bhuee  
 GET: Ricardo Perez  
 IBI Group: David Nicholas, Jennifer Moore, Angela Jarvis, Oliver Hartleben, Aamir Ansari, Ali Lewis



#### Intent

The intent of the stakeholder outreach and workshop was to engage a cross-section of the community to gather input, identify streetscape preferences and open a dialogue about the potential for Downtown Bakersfield. The insights gained through this process will inform the design theme of the Streetscape Master Plan for Downtown Bakersfield.

#### Process

Three exercises were held with 11 participants from the City, Chamber of Commerce, business representatives and community groups. The goal of the exercises was to rank objectives that the plan can support and execute, gather visual preferences of streetscape elements, and identify areas of Downtown Bakersfield where the most impact can be made.

#### Agenda

The workshop consisted of three exercises to provide design direction for the preparation of the master plan.

- The group validated the Design Objectives into the predetermined categories Essential, Important, Good to Have and Nice to Have.
- Preference Survey involved the group selecting material preferences similar to the online survey but with a greater number of options. This exercise provided design direction for physical design materiality.
- Mapping Exercise involved the group identifying priority areas for activity and gateways. The group was asked to identify areas that already have a hum that can be used as a catalyst to build on.

#### Mapping Exercise

The goal of this exercise was to hear from the group which areas of Downtown Bakersfield were already successful and which ones were up and coming (emerging). We also asked the group to pinpoint areas that they thought were gateways, both on a larger scale and a smaller scale.



#### Design Objectives – Essential

The Essential Top 4 design objectives that have been discussed among stakeholders



#### Design Objectives – Important

The important design objectives filling spots 3-8 among stakeholders



#### Design Objectives – Good to Have

The good to have design objectives filling spots 9 & 10 to be considered in the plan





### Public Art

Public art may fall into any of the three streetscape component categories defined in this document; hardscape, vertical features, and/or furnishings. The proposed strategy is to build on the existing efforts and offer additional locations and ways in which the local culture may be celebrated through additional public art opportunities.



Mural - Eye Street & 20th Bakersfield, CA



Mural - Chester Avenue & Service Street Bakersfield, CA



Sculpture should reflect the interests of the community.



Sculpture may be on the ground plane or overhead but lighting is encouraged for 24 hour interest.



Bike racks in open spaces or at key tourist destinations can contribute to the local story.



Statues can bring life to the streetscape and celebrate local characters. Precedent: Beatles Statue, Liverpool, England



Public Art may be clad to otherwise utilitarian building facades to create interest.

Precedent: Hands by Christian Moeller, San Jose Airport, CA. Created with plastic pixels affixed to architectural mesh.



Interactive art installations function provide a visitor attraction suitable for urban plazas.

**Neighborhood and/or District Signs** should be large scale and have a consistent design language. The signs need to be at a scale that is legible from both vehicles and pedestrians and is discernible from a distance. The sign should include "Bakersfield" as well as the district or neighborhood name, for example "Arts District". Specific locations should be determined through a wayfinding and signage plan.



Neighborhood/District Signs



Neighborhood/District Gateway Signs  
Precedent: Sacramento, CA

#### Sign Concept

Bakersfield - "The Sound of Something Better"  
3 Dimensional Soundwave of the song "The Streets of Bakersfield". To be further developed into a columnar neighborhood/district sign.  
Artist: IBI Group



Overpass Entry Gateway  
Precedent: De Zwerm Underpass Eindhoven, The Netherlands  
Photo Credit: Frank Tjepkema, Studio Tjep



Overpass/Bridge Entry Gateway  
Precedent: Gateway WA Perth Airport Perth, Australia  
Photo Credit: Jun Kendrick, Artist Liaison



**Overpass/Bridge Gateways** can be an opportunity for dramatic public art or signage in areas where they are City owned or partnerships can be negotiated. Opportunities would need to be reviewed on a case by case basis.

Overpass/Bridge Gateway  
Precedent: West Schantz Avenue Bridge, Kettering, Ohio  
Photo Credit: www.playkettering.org  
Artist: Vicki Scuri SiteWorks  
Engineering: IBI Group



## Plazas & Pocket Parks

The evolution of Bakersfield will present opportunities for additional public spaces. Small city-owned parcels throughout the downtown area that are adjacent to public uses or otherwise unsuitable for development provide opportunities to either provide small gathering spaces or improve the streetscape aesthetics. These public spaces may be programmed or they may simply function as extensions of the pedestrian realm within the public right-of-way.



Plazas and Pocket parks are an opportunity for the unexpected and to provide comfort and amenity in small urban spaces.



Water features are at home in urban plazas and pocket parks to provide cooling and public realm activation. Consideration should be given to be given for water conservation and minimal water use in any water feature.



Precedent: Formosa Pocket Park, designed by Katharine Spitz Associates, West Hollywood, CA is an example of infill public space, built over a private parking garage.



H Street & 4 Street - the triangle shaped island at this intersection is an example of found open space. While the location does not lend itself to being a functioning park, it has potential to improve the urban ecology, streetscape aesthetics and overall pedestrian experience. Adding a gateway feature and/or public art element would improve the aesthetics of the area.



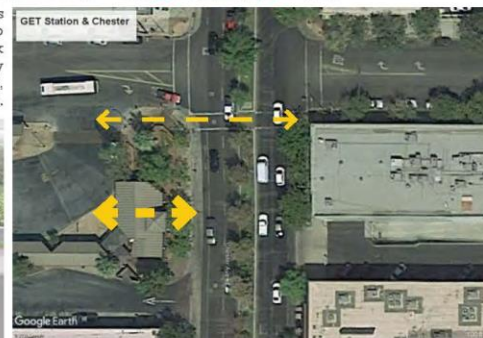
Precedent: Urban Plaza with Art Installation - Parque De Los Pabladores - SOFA District, San Jose CA

Plaza is a triangular space of land similar in scale to H Street and 4th. This precedent serves as an example of found open space and public realm improvement.

GET Station - as a redeveloped transit station, there is an opportunity to open up the station waiting area to Chester Avenue, providing an extension of the sidewalk and allow the station to function as an urban plaza. By opening up views and circulation in and out of the site, function and safety will be improved.



Precedent: West Harbour Go Station Plaza, Hamilton, ON, Canada. Bus and Train Station with entry plaza community space.



## Street Trees

Three different zones have been identified to organize the street tree plantings along Chester Avenue and H Street, as illustrated in Exhibit 8. Street tree plantings are grouped into three types based on factors such as adjacent land-uses and block sizes. The three zones include the Downtown Core, Downtown Edge, and a Residential Zone. Each zone contains unique tree sizes, species and spacing that help define the streetscape experience.

Exhibits 9 - 20 demonstrate *typical examples* of how to apply the tree requirements on segments throughout the corridor. Consideration shall be given to site lines, driveways and business signage in final tree placement & spacing.

### Zone A: Downtown Core 20th Street to Truxtun Ave

This zone incorporates the area with the highest land-use density and pedestrian activity.

- Trees spaced 25' on center
- Tree size: Medium (see planting matrix for list of medium street trees)
- Median trees (if applicable): 30' on center

### Zone B: Downtown Edge Golden State Ave to 20th Street & Truxtun Ave to 4th Street

This zone incorporates the outer downtown areas, which is less dense than the "Core" area, but still represents an urban condition.

- Trees grouped in pairs, spaced 35' on center
- Tree size: : Large (see planting matrix for list of large street trees)
- Median trees (if applicable): 30' on center

### Zone C: Residential 4th Street to SR-58

This zone incorporates the area that is largely defined by surrounding residential uses.

- Trees grouped in threes, spaced 20' on center
- Tree size: Medium (see planting matrix for list of medium street trees)
- Median trees (if applicable): 30' on center

### Side Streets (Non Chester Ave. or H St)

- Trees on all side streets spaced 30' on center



Exhibit 8: Street Tree Zones

# Chester Avenue Concept (Truxtun Ave to SR58)

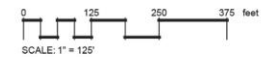
## STREETSCAPE IMPROVEMENTS



TRUXTUN AVENUE TO 6TH STREET



6TH STREET TO SR-58



# H Street Concept (Truxtun Ave to SR58)

## STREETSCAPE IMPROVEMENTS



TRUXTUN AVENUE TO 6TH STREET



6TH STREET TO SR-58

